

**CULTURAL HERITAGE IMPACT ASSESSMENT
10020 MISSISSAUGA ROAD**

**EAST PART LOT 11, CONCESSION 5
WEST OF HURONTARIO STREET
GEOGRAPHIC TOWNSHIP OF CHINGUACOUSY**

**CITY OF BRAMPTON
REGION OF PEEL, ONTARIO**



**August 2016
Revised November 2016**

**Prepared for:
R.V. Anderson Associates Limited**

Prepared by:



UNTERMAN McPHAIL ASSOCIATES
HERITAGE RESOURCE MANAGEMENT CONSULTANTS

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1.0 INTRODUCTION TO THE DEVELOPMENT SITE

1.1 Purpose of Study

R.V. Anderson Associates Limited, on behalf of the Region of Peel, retained Unterman McPhail Associates, Heritage Resource Management Consultants, to undertake a Heritage Impact Assessment (HIA) for the property located at 10020 Mississauga Road. The Region of Peel is undertaking improvements along Mississauga Road from North Bovaird Bridge West to Mayfield Road, City of Brampton, in accordance with the recommendations of the Class EA completed in April of 2013 for this project.

The subject property located at 10020 Mississauga Road is listed in the City of Brampton's *Municipal Register of Cultural Heritage Resources* (February 2016 April 2016). Accordingly, the City of Brampton has requested the preparation of a HIA for the listed property.

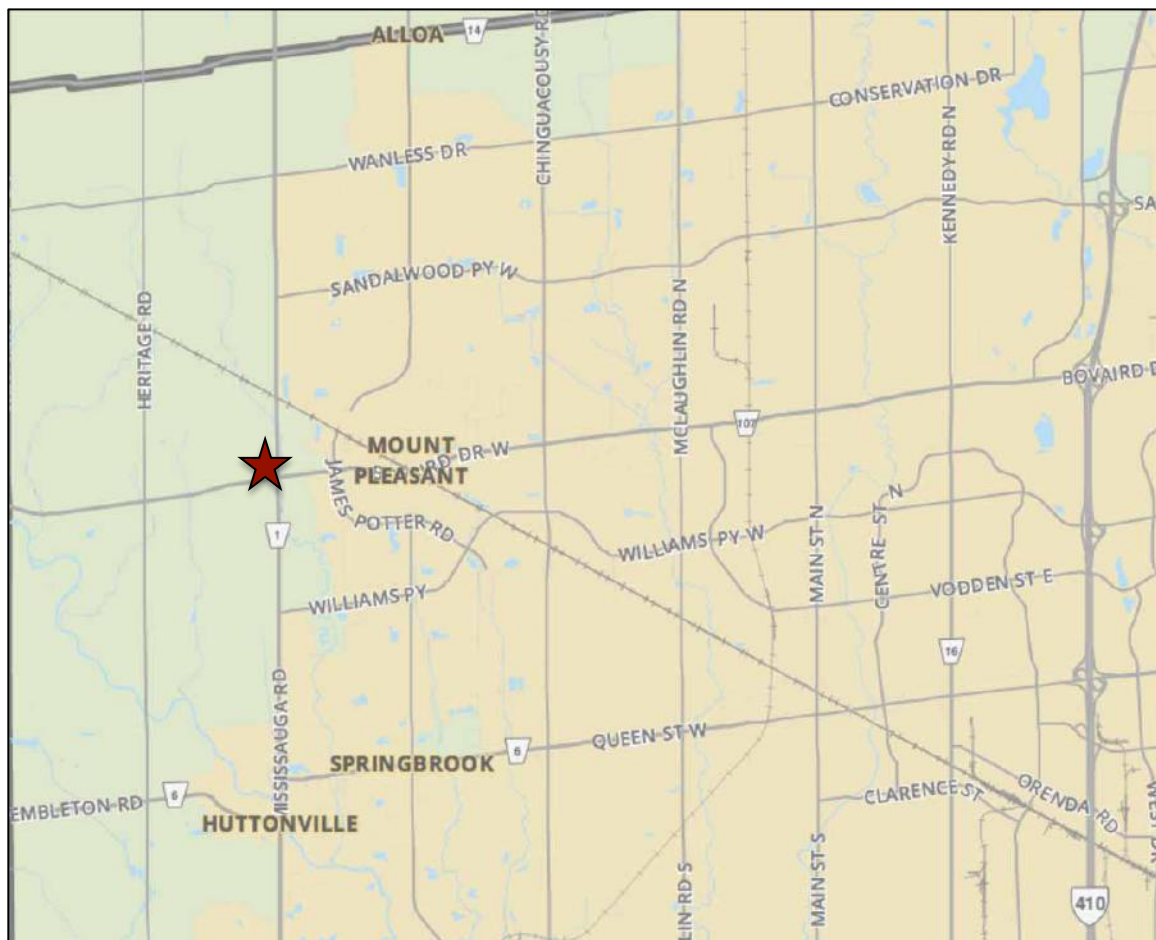


Figure 1. Location of 10020 Mississauga Road, City of Brampton [Region of Peel, Peel Data Centre, February 2016].

1.2 Description of the Property

The subject property is situated on the northwest corner of the intersection of Mississauga Road and Bovaird Drive West in the City of Brampton (**Figure 1**). Specifically, it is situated on the east part of Lot 11, Concession 5 WHS of the geographic Township of Chinguacousy. The subject property includes a c1880 brick residence at 10020 Mississauga Road, which is located on the commercial property known as The Apple Factory Farm, municipal address 10024 Mississauga Road. As well, there are two mid 20th century residences located at 10042 and 10054 immediately north of the brick residence and a large storage building on the west side of the property. The principal built heritage resource on-site is the c1880 brick residence, which it is currently vacant. The surrounding lands, once rural in character, are in transition to urban use.

The subject site is located within the City of Brampton Huttonville North Secondary Plan (Area 52), now part of the Heritage Heights Community. Heritage Heights is bound by Mayfield Road to the north, Winston Churchill Boulevard to the west, Mississauga Road to the east and the Credit River Valley to the south.

1.3 Ownership and Legal Description

The legal description of the property is Part of Lot 11, Concession 5 WHS, geographic Township of Chinguacousy, designated Part 1, Reference Plan 43R-11013 (PIN 14380-5934), City of Brampton.

The owner of the subject property is:

The Apple Factory (2007) Corporation
7045A Tomken Road
Mississauga, On L5S 1R7
Attention: Mr. James D. Picard

1.4 Report Format

This HIA is intended to fulfill the heritage planning requirements of the City of Brampton. This report follows the terms of reference *Heritage Impacts Assessment – Terms of Reference* prepared by the Planning, Design and Development, Heritage, City of Brampton. The Ministry of Tourism, Culture and Sport (MTCS) information sheet series for Provincial Policy Statement (PPS 2005), specifically *InfoSheet #5: Heritage Impact Assessments and Conservation Plans* (2006), were also consulted.

The report includes:

- an introduction to the development site (Section 1.0),
- a historical summary of the subject property at 10020 Mississauga Road (Section 2.0),
- a description of the subject property and its context (Section 3),

- a description of the built heritage resource description (Section 4.0),
- An evaluation process and the Statement of Cultural Heritage Value (Section 5.0),
- a description of the development proposal (Section 6.0),
- a summary of relevant heritage policies (Section 7.0),
- an assessment of the impact of the proposed development (Section 8.0), and
- a discussion of conservation and mitigation recommendations (Section 9.0).

Appendix A includes a selection of historical maps and photographs. Photographs of the cultural heritage landscape and the built heritage resource are found in Appendix B and Appendix C, respectively. On-site fieldwork was undertaken in March 2016. Unterman McPhail Associates completed digital photography of the subject site and residence presented in this report at that time.

2.0 HISTORICAL SUMMARY

2.1 Introduction

In 1788, the district of Montreal was divided into four administrative districts named Hesse, Nassau, Mecklenburg, and Lunenburg. These four original district names were changed in 1792 with Hesse becoming the Western District, Lunenburg the Eastern District, Mecklenburg the Midland District, and Nassau the Home District. Several new districts were established and the old ones were changed in 1798. In 1800, the districts were divided into counties. Around the same time the boundaries of the various townships, counties and districts were regularized so each township was contained in a single county, and each county in a single district. The Home District, which contained the York (Toronto) and York County, was divided into four ridings.

Upper and Lower Canada united to form the Province of Canada in 1842 and Upper Canada became known as Canada West. Canada West was divided into twenty districts, which were then subdivided into counties, ridings and townships. As the large districts were settled and their populations grew, they were subdivided into newer and smaller districts. County governments took on the district responsibilities when the districts were abolished in 1849. In the Home District in 1851, the West Riding, consisting of the Townships of Albion, Caledon, Chinguacousy, Toronto Gore and Toronto, was established as Peel County. A council made up of members from the United Counties of York and Peel governed Peel between 1851 and 1866. In 1867, Peel County became a separate governmental entity.

2.2 Township of Chinguacousy

The British government bought 33,995 hectares (84,000 acres) of the "Mississauga Tract", often referred to as the First Purchase, in 1805 from the Mississaugas. Chinguacousy Township was surveyed in 1819 on a grid survey with north to south concession lines set three-quarters of a mile apart and east to west sideroads set two miles

apart. Each rectangle was divided into five lots of 200 acres of land. The lots were generally sold as half lots of 100 acres. Hurontario Street, or the Centre Road, divided the township into two parts with the concessions numbered to the east and to the west from the road.

The initial clearing and settlement period began soon after the survey. By 1827 the population of Chinguacousy approximated 223 people and 27,211 acres of land, or about one-third of the total acreage, had been taken up and 3,702 acres had been cleared.¹ Early settlers built log dwellings, cleared land for subsistence agricultural crops and for livestock pastures and cleared the surveyed road allowance along their property boundary.

In the 1830s, land clearance accelerated due to a large influx of immigrants from the British Isles. *Smith's Canadian Gazetteer* (1846) described Chinguacousy Township as,

*...74977 acres taken up, 26,266 of which are under cultivation. This is one of the best settled townships in the Home District, containing excellent land, and many good farms....Excellent wheat is grown in this and the adjoining townships. Population in 1842, 3965.*²

The Census Return (1851) indicates dwellings in Chinguacousy Township outside of the village of Brampton were for the most part of log construction with some brick and frame and a few of stone houses. The economic boom associated with the wheat production of the 1850s helped to create a prosperous farming community in the township. *Tremaine's Map* (1859) depicts a developed agricultural landscape with open concession roads and sidelines, many farmsteads on lots that were typically divided into east and west parts.

Transportation in the township was improved from the mid 19th century onwards. The Hurontario Plank Road Company began improvements to Hurontario Street (Centre Road) through the township and Brampton in the late 1840s and the local road system was improved in the latter part of the 19th century by the township. The Toronto and Guelph Railway was issued a charter in late 1851 and a proposed line was surveyed through from to York (Toronto) to Guelph York passing through Brampton and Chinguacousy Township. This railway was merged with the Grand Trunk Railway (GTR) in 1853 while under construction; the GTR was opened through Chinguacousy Township in 1856. Access to a railway provided greater access to larger and more distant markets for local agricultural products. In 1877, the Credit Valley Railway (CVR) was completed through Brampton and Chinguacousy Township. The Canadian Pacific Railway (CPR) acquired the line in 1884.

¹ *A History of Peel County To Mark its Centenary as a Separate County 1867-1967* (The Corporation of the County of Peel, November 1967) 246.

² Wm. Smith, *Smith's Canadian Gazetteer* (Toronto: Wm. H. Smith, 1846) 32.

The Census Return (1861) still recorded numerous log houses in rural Chinguacousy Township; however, there was also a noticeable increase in the number of brick and frame houses as well as a small increase in the number of stone houses constructed during the 1850s. This use of more substantial and permanent construction material marks a movement beyond subsistence farming to well-established and prosperous farmsteads created by the prosperity of wheat production. More commodious and substantial barns were also built alongside the new farmhouses.

The Chinguacousy Map in the *Illustrated Historical Atlas* (1878) indicates a well-established agricultural landscape with many farmsteads, often with orchards, churches, schoolhouses and small hamlets. The Atlas describes Chinguacousy Township as,

*...a first class agricultural township and the farmers as a general thing have been successful in their undertakings, many of them have amassed quite a fortune. The township is noted for its beautiful and substantial farm residences and commodious barns. The farms also are generally in the highest state of cultivation, while the grounds in front of the residences are for the most part tastefully arranged with beautiful flowers and shade trees, giving each place and the country generally a handsome appearance.*³

As the wheat economy of Peel County ended by the 1880s, local agriculture adapted and continued to prosper throughout the remainder of the 19th century and into the 20th century. Family farms were retained and worked with increasing mechanization. Second and third generation improvements included wind breaks, fence lines, hedgerows and maintained road allowances. New and larger farmhouses and barns were built. This agricultural economy continued throughout most of the 20th century with apple growing established as an important agricultural crop. Twentieth century topographic maps and aerial photographs indicate the area west of Brampton remained largely agricultural in character with little subdivision for most of the century. By the early 1900s a hamlet had developed at Mount Pleasant.

In 1920, the Ontario government adopted the Provincial Highway System and assumed the Sarnia-Guelph Road as a provincial highway. In 1921, the highway was extended easterly to Brampton as was known as the Sarnia-Brampton Highway until 1925 became Provincial Highway 7. By 1927, the highway section from Georgetown to Brampton had been paved. Downloaded to the municipality in the late 1990s, Highway 7 was renamed Bovaird Drive West in Brampton.

After the Second World War, larger farms changed by enlarging fields, and removing tree lines, hedgerows and fence lines and materials. The farm complexes were characterized by renewal of older agricultural buildings with specialized barn structures and tall, large, modern silos. When the Regional Municipality of Peel was established on January 1, 1974, the new City of Brampton included part of Chinguacousy Township.

³ *Illustrated Historical Atlas of the County of Peel, Ont.* (Toronto: Walker & Miles Co., 1877) 64.

Urban development began to transform the agricultural landscape of the township outside of the city centre in the late 20th century, largely in the southern part, then the east, and finally the west. Subdivisions for single detached homes occurred intermittently along township roads. Commercial development began to occur such as a gasoline station and The Apple Factory at Bovaird Drive West and Mississauga Road. In the Mount Pleasant area, a GO-Transit station and mixed use node was developed in the early 21st century along with housing subdivisions. Residential subdivisions were developed north of Bovaird Drive West as well as east and west of Creditview Road to just below Wanless Drive. Currently residential subdivisions are being built on the east side of Mississauga Road between Bovaird Drive West and Mayfield Road.

2.3 10020 Mississauga Road

2.3.1 1824-1839

Land records indicate Alex. C. Lawrence received the Crown Patent for all 200 acres of land in Lot 11, Concession 5 WHS, Chinguacousy Township, Peel County in May 1824. Lawrence sold the northeasterly and southwest half of Lot 11 to Archibald McNichol in 1828, and McNichol sold the southwest half to Timothy Greensword and the east half of Lot 11 to James Witham in 1839.

2.3.2 1839 to 1880

James Witham sold a seven acre parcel of land on the east half of Lot 11 to John Ewing in November 1839. The Assessment Roll (1843) notes John Ewing was located on seven acres of land on Lot 11, Concession 5 WHS. In June 1846, Ewing sold the seven acres to John Anthony.

Meanwhile, Witham also subdivided a one acre parcel of land on the east half of Lot 11 and sold it to John Green in August 1848 for £15. Two years later in November 1850, Green sold the one acre property along with other land on the west half of Lot 11, Concession 5 WHS, to William Anthony. Mark Anthony registered a quit claim to transfer interest in the real property to Joseph White in July 1869 for a one acre property on the east half of the lot and White and his wife sold the one acre to Margaret Brien in March 1871 for \$100.00.

Tremaine's Map (1859) notes J. A. [James Anthony] was the owner of the southeast corner of Lot 11. The map does not indicate a building in the southeasterly corner of Lot 11 at this time. The last will and testament for John Anthony was registered in 1863. Township assessment rolls recorded Francis Anthony on eight acres of land on Lot 11, Concession 5 WHS. Land records show Mark Anthony and his wife sold seven acres on the southeast part of Lot 11 to James Anthony in September 1867. Two years later, in April 1869, James Anthony sold the seven acres to Joseph White and Isabelle White. The Whites sold the seven acres to Margaret Brien in March 1871 for \$536.00.

By March 1871 Margaret Brien of Chinguacousy owned both the seven acre and one acre land parcels in the southeasterly corner of Lot 11. The township assessment roll for 1871 indicates George Brien, the husband of Margaret Brien, owned eight acres of land on Lot 11, Concession 5 WHS and was a householder on another property. The county directory for 1873-74 indicates Charles “Breen” [Brien] owned property on Lot 11, Concession 5 WHS. Land records show Margaret Brien leased the eight acres, described as being in ‘the southeasterly corner’, for \$50.00 per annum to Nathaniel Logan in 1874. The *Illustrated Historical Atlas* map (1877) notes Mrs. B. (Margaret Brien) as a non-resident owner of the southeast corner of Lot 11. An orchard is shown on the subject property; however, the map still does not show a building on the southeast corner of Lot 11. In November 1880, Margaret Brien sold the eight acres to James H. Scott for \$600.00.

The David McClure family owned the remainder of the east half of Lot 11, Concession 5 WHS by 1852.

2.3.3 1880 to 1914

James Hunter Scott was born in Chinguacousy Township on December 28, 1844⁴, the son of John Scott and Mary Hunter. His father John Scott was born in Armagh, Ireland in 1814, and with his family emigrated from Ireland to the British colony and settled in Chinguacousy Township during its early development years. Likewise, his mother Mary Hunter was born in Ireland in 1819 and emigrated with her parents Joseph and Jane Hunter and settled in Chinguacousy Township.

James H. Scott married Mary Amanda Scott on March 26, 1872. Mary A. Scott was born in Chinguacousy Township on July 28, 1853, the daughter of William G. Scott and Margaret Graham.⁵ The Census Return (1881) notes James and Mary Scott lived in a single household in Chinguacousy Township. James was recorded as a farmer by profession. The census return (1881) does not include information on dwellings and the township assessment rolls for the early 1880s do not exist. However, the township assessment roll (1881) records James H. Scott as a tenant on 95 acres of land on west part of Lot 13, Concession 5 WHS, the Estate of James Anthony, and the owner of eight acres on Lot 11, Concession 5 valued at \$300.00.

It would seem probable the present brick house on the subject property was built in the early 1880s for the Scotts after their purchase of the eight acre property. Local directories (1888 and 1890) note James Scott was a resident on Lot 11, Concession 5.⁶ The Census

⁴ Reference to a birth date of December 28, 1843 was also noted on records.

⁵ AO, Ontario, Canada, Marriages, 1801-1928, Peel County, Chinguacousy Township, 159, James Scott and Mary Amanda Scott, March 26, 1872. Mary Scott’s mother’s surname is shown as Graham on the marriage certificate of Mary A. Scott and James H. Scott and as Lee on the death certificate of Mary A. Scott.

⁶ *Union Publishing Co.’s Farmers’ and Business Directory for the Counties of Dufferin, Halton, Ontario Peel and York for 1888*, Vol. V (Ingersoll, Ontario: Union Publishing Co., of Ingersoll, 1988) 187; and,

Return (1891) notes James H. Scott, whose profession was noted as “marketman”, along with his wife Mary lived in a two storey brick house with eight rooms. Although the agricultural return for 1891 does not exist and the exact location of the Scotts is not recorded, it is believed the brick house is the current one on the subject property since David McClure is the next entry in the return recording households.

Ten years later, the Census Return (1901) notes farmer James Scott and his wife Mary Scott were the occupants of a nine room brick house. The Scotts owned a total of 58 acres of land including the eight acres in the southeasterly corner of Lot 11, Concession 5 WHS, one house and three barns or outbuildings. Twentieth century topographic maps and aerial photographs show a barn associated with the brick house also stood on the southeast corner of Lot 11, Concession 5 WHS.

The Census Return (1911) records James Scott, wife [Mary] Amanda and Rosie Scott, as a household located on Lot 11, Concession 5 WHS. Rosie Scott, noted as an adopted daughter, was born in England in May 1896 and arrived in Canada in 1904. James Scott is recorded in a 1911 directory as the owner of property on Lot 11, Concession 5.⁷

Land records show James and Mary Scott sold the eight acres of land on Lot 11, Concession 5 WHS to Jessie and Robert Taylor in January 1914 for \$2500.00. The Scotts moved to the village of Norval around this time, where they lived until their deaths. James Hunter Scott died in 1928 and Mary A. Scott in November 1934, both at their home in Norval.⁸ Both are buried in Hillcrest Cemetery, Georgetown.⁹

2.3.4 1914 to Present

Several people owned the subject property on Lot 11, Concession 5 WHS with the brick residence in the early to mid 20th century. John Taylor is noted as the owner of Lot 11, Concession 5 WHS in Chinguacousy Township in 1915.¹⁰ Land records indicate Jessie Taylor and others sold the eight acres of land to George Green in October 1915. Two years later, in April 1917, John Poyntz bought the eight acres from Green for \$2800.00.

John Poyntz, a local farmer situated on Lot 12, Concession 4 WHS, owned the subject property for seven years between 1917 and 1924. He sold the eight acre property to Mary Dowell in April 1924 for \$1.00. Sarah A. McClure and Joseph B. McClure, as joint

The Union Publishing Co.'s (of Ingersoll) Farmers' and Business Directory for the Counties of Dufferin, Peel and York for 1890, Vol. VI (Ingersoll, Ontario: Union Publishing Co., of Ingersoll, 1890) 182.

⁷ *The Union Publishing Company's Farmers' Directory for the Counties of Dufferin, Halton, Peel, Waterloo and Wellington York 1911*, Vol. XIX (Ingersoll, Ontario: Union Publishing Co., of Ingersoll, 1911) 132.

⁸ AO, Ontario, Canada, Deaths, 1869-1938, Peel County, Chinguacousy Township, #017103, James Hunter Scott, September 23, 1928; and, #016989, Mary Amanda Scott, November 18, 1934.

⁹ Find A Grave, Mary Amanda Scott, Access: --<<http://www.findagrave.com/cgi-bin/fg.cgi?page=g&r&GRid=62357599&ref=acom>> (February 2016).

¹⁰ *Vernon's Farmers and Business Directory for the Counties of Dufferin, Halton, Peel, Waterloo & Wellington for the Year 1915* (Hamilton, Ontario: Henry Vernon & Sons, 1915) 127.

tenants, bought the property from Mary Dowell in October 1926 for \$1.00 and consideration. The McClures owned the subject property for fifteen years between 1926 and 1941.

Helen L. and Hudson S. Reid, as joint tenants, acquired ownership of the subject property from Sarah A. McClure in May 1941 for \$3000.00. Six years later, the Reids sold the property to Marion G. Eccles and Agnes Eccles, who were the owners between 1947 and 1953. Archie Fuller acquired ownership from Marion (Eccles) Dolson and Agnes Eccles in 1953. He sold the property to Roy Laidlaw in the latter part of the 20th century.

Various generations of the Laidlaw family owned and operated farms in Chinguacousy Township from the 19th century onwards. Roy and Lenore Laidlaw and their son Clifford Laidlaw started the idea of The Apple Factory in 1979 when Roy Laidlaw, a local apple grower, acquired the eight acre property including the brick house at 10020 Mississauga Road. The Laidlaws built The Apple Factory, now 10024 Mississauga Road, as a commercial outlet to sell their produce. Soon after opening their business venture, the store was expanded to sell other fruits, vegetables and crafts and to include a bake shop and a butcher shop.

Roy and Lenore Laidlaws continued to live on their farm complex on Heritage Road while son Clifford and his wife Deborah Laidlaw occupied the subject residence at 10020 Mississauga Road. Eventually a gift shop and chocolate factory operated in the brick residence. The Laidlaws sold the business and property to the current owner, known as The Apple Factory (2007) Corporation, in 2007. The sale of the property was registered on title in 2012.¹¹ From 2005 to c2010, the residence was used as a real estate office, and from 2010 to the present it has been vacant.

3.0 DESCRIPTION OF THE PROPERTY AND ITS CONTEXT

3.1 Location

The subject property at 10020 Mississauga Road is located in the northwest corner of the intersection of Mississauga Road and Bovaird Drive in the City of Brampton. The property comprises eight acres of the southeast corner of Lot 11, Concession 5 WHS of the geographic Township of Chinguacousy, currently Part 1, RP 43R-11013.

3.2 Area Context

The study area is located in the physiographic region known as the Peel Plain, an undulating tract of clay soil that slopes gradually towards Lake Ontario. The Credit River

¹¹ “The Apple Factory a throwback to a simpler time”, Brampton Guardian (January 18, 2016).
Access: --< <http://www.bramptonguardian.com/community-story/6231142-the-apple-factory-a-throwback-to-a-simpler-time/>> (February 2016).

crosses the plain. The underlying geologic material is a till containing large amounts of shale and limestone modified in much of the area in the Peel Region by a veneer of clay.

Chinguacousy Township was opened for agricultural settlement in the early 1800s. Historically, Mississauga Road was surveyed as the concession road between Concession 5 and 6 to the west of Hurontario Street and Bovaird Drive West as a sideline between township Lots 10 and 11. After the original hardwood forest was cleared during the initial settlement period the township land was transformed into an agricultural landscape. Small population centres were established to provide services to the local rural communities. The proximity of Brampton, the county seat, allowed the residents to access a broad range of manufacturers, professional services and tradesmen. By the mid 19th century the township was a noted wheat-growing area; later mixed farming developed to replace the wheat economy. Railway access was available by the mid 19th century and local roads were much improved in the early 20th century. The area sustained a well-developed agricultural economy into the latter part of the 20th century.

King's Highway 7, acquired in 1921 as a provincial highway, was downloaded to the City of Brampton in the late 1990s and renamed Bovaird Drive West. The Mount Pleasant GO Station is a transit railway station that is located to the east at Bovaird Drive West and Ashby Field Drive in Mount Pleasant.

The land along Mississauga Road north and south of Bovaird Drive West remained, for the most part, in agricultural use into the late 20th. More recent development has resulted in a loss of farm complexes and agricultural land on the east side of Mississauga Road. On the west side of the road to the north of the railway line, the former rural character has been considerably altered in preparation for development. South of the railway line to Bovaird Drive West, the land is under development planning initiatives.

3.3 Site Description

The brick residence on the subject property is the principal heritage resource on the subject site. It is located on the west side of Mississauga Road just north of its intersection with Bovaird Drive West on The Apple Factory site (**Figures 2**). Specifically, it is located on Lot 11, Concession 5 WHS of the geographic Township of Chinguacousy, City of Brampton.

Mississauga Road on the east property boundary is part of the Peel Region transportation system. This north to south, two-lane paved road follows a straight alignment in proximity to the subject property. There is a traffic light and a left turn lane at its intersection with Bovaird Drive West. To the north of the railway track crossing, the west side of the road has been graded to accommodate four lanes of traffic. Bovaird Drive along the south property boundary is a regional arterial road with four lanes of traffic at Mississauga Road. The land surrounding Mississauga Road is in transition. The southwest corner includes a current service station. The Huttonville Creek, part of the Credit River system, runs on the east side and parallel to Mississauga Road. The

McCandless House property on the southeast corner of the intersection of Bovaird and Mississauga Road is owned by the City of Brampton.



Figure 2. The residence at 10020 Mississauga Road is situated on the west side, and close to, the well-travelled arterial road on the north side of a site entrance to The Apple Factory.

The brick residence, which faces east onto Mississauga Road, was built in the early 1880s and is associated with James H. and Mary A. Scott. The building was used as a residence from the early 1880s into the latter part of the 20th century, when it was converted to commercial use. It has been vacant since c2010. An aerial photograph from 1954 (**Figure 3**) shows the subject property and its surrounding rural environs with an associated barn structure to the west of the residence. Access from Mississauga Road was on the north side of the house (**Figure 4**). Numerous trees were located in the vicinity of the house. The development of The Apple Factory in the 1990s transformed the character and immediate setting of the house.

Currently, the brick house sits on the east side of The Apple Factory property. To its north there is side yard of grass and a paved parking lot for commercial vehicles. Two mid 20th century residences located at 10042 and 10054 Mississauga Road to the immediate north of the brick residence are accessed from Mississauga Road on the north side of the subject residence. A Canada Post box site is located in front of 10042 Mississauga Road in the paved drive/parking area. Immediately south of the brick

entrance to The Apple Factory site from Mississauga Road. A large paved area for customer parking extends from the south side of the brick house to Bovaird Drive and west to the side and front of The Apple Factory building. To the west of The Apple Factory store is another large paved area for delivery and commercial vehicles and a large storage building. Access to the site is also available from Bovaird Drive West.

Situated on a corner lot, the subject brick house is a visual landmark travelling west on Bovaird Drive West towards Mississauga Road and travelling north on Mississauga Road to Bovaird Drive West (**Figure 4**) and a familiar landmark due its association with The Apple Factory, a popular local food market.

Additional photographs of the site context are contained in Appendix B.



Figure 3. This aerial photograph (1954) shows the subject property at 10020 Mississauga Road on the northwest corner of the intersection with Highway 7, now Bovaird Drive West [UofT, Digital Aerial Photographs. Southern Ontario 1954, Hunting Survey Corporation Limited, #436.794].



Figure 4. An annotated aerial photograph depicts the subject property on the northwest corner of the current Bovaird Drive West and Mississauga Road, City of Brampton [Bing Maps, Microsoft 2016, as adapted].

4.0 BUILT HERITAGE RESOURCE DESCRIPTION

The principal built heritage resource at 10020 Mississauga Road is a one and-a-half storey, detached brick residence believed to have been built in the early 1880s. Evidence to support this construction date is found in the historical records, the design of the structure and its physical fabric.

The following description is based on a site visit undertaken in March 2016 that included access to the exterior and ground and second floor of the house interior. The building is currently vacant and in need of maintenance and roof repair. A description of the exterior of the building is included in *Section 4.1* and the interior in *Section 4.2* and modifications to the building in *Section 4.3*. A basic number of exterior dimensions were taken to aid in the description of the building. Imperial units were used; metric equivalents are provided in brackets.

A selection of exterior and interior photographs of the building taken in March 2016 are found in Appendix C.

4.1 Residence: Exterior



Figure 5. A view of the front elevation of the residence at 10020 Mississauga Road depicts the centre gable roof with vergeboard and the decorative full width verandah.

The one and-a-half storey red brick building located at 10020 Mississauga Road is a vernacular example of Gothic Revival architectural style from the latter part 19th century (**Figure 5**). Typical Gothic Revival elements include the cross gable roof with centre gables, a one storey bay window and dichromatic brickwork. The original building layout comprises a main section and a rear brick wing to the west side that forms a 'T'-shaped plan. A one storey addition with a garage was added to the south and west elevations of the house in the latter part of the 20th century.

Set close to Mississauga Road, the front (east) elevation of the main section comprises two bays rather than a more typical three-bay composition. The main section has a rectangular plan that measures approximately 28-ft. (8.53 m) north to south and 18-ft. 1-in. (5.51 m) east to west. The 'T'-shaped rear wing measures 31-ft. 1-in. (9.47 m) east to west and approximately 13-ft. 3-in. (4.04 m) north to south.¹² On the north side, the rear wing is set back about 5-ft. 5-in. (1.65 m) from the north wall of the main section and approximately 9-ft. (2.74 m) from the east wall of the main section. The modern one storey addition that wraps around the south and west wall of the rear wing includes a one-car garage on the west elevation and an enclosed interior space on the south elevation. The addition has a shed roofline and is covered with modern siding. It measures about 23-ft. 10-in. (7.26 m) north to south and 31-ft. 1-in. (9.47 m) east to west.

The brick walls of the main section and the rear wing are set on a rubblestone foundation wall and laid in a running bond. The masonry joints do not display any distinctive tooling. The brick masonry units in the main section and the rear addition measure about 8-in. ¼ -in. (209.55 mm) in length, 3-¾ -in. (95 mm) in width and 2-½ -in. (64 mm) in height. The foundation wall appears to be more regularly coursed on the south elevation of the main section than on the north wall. Visible existing basement window openings (north and south walls) are set in the foundation wall for the main house. A former basement window opening is located on the north elevation of the rear wing.

The cross gable roof of the main section gable roof of the rear wing projects over the masonry walls. A chimney was removed from the west end of the roof ridge of the rear wing as shown in a 2005 photograph (**Appendix C**).

The door and window openings of the main section and the rear wing are segmental in shape and feature wood lug sills. They include single openings, a bay window and paired narrow window openings. The openings feature a dichromatic buff brick accent that runs across the top and down the sides of the opening like a label. The top of the horizontal row of bricks has a slightly projecting row of narrow bricks ends that add dimension and visual interest (**Figures 7 and 8**). The basement windows on the north side of the main section have radiating red brick voussoirs while the basement window in the bay on the south elevation of the main house has a radiating buff brick voussoir.

¹² The approximate east to west measurements and the set back of the south wall of the rear addition are calculated from interior wall measurements due to the exterior addition on the ground floor.



Figure 6. This view of the south elevation shows the 'T'-shaped rear wing extending to the west of the main section and the late 20th century addition.

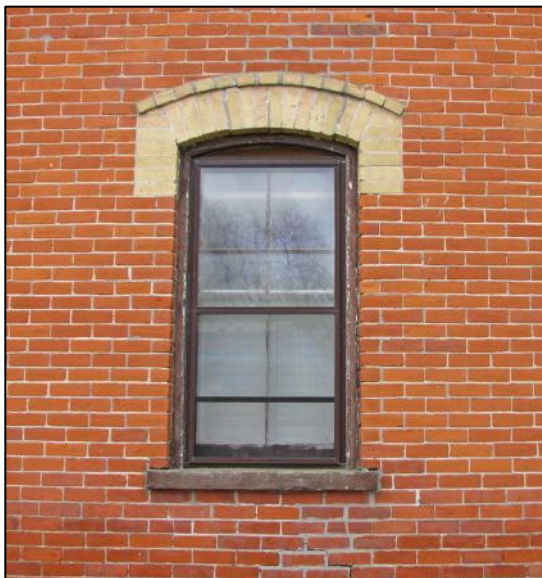


Figure 7. This view shows a typical segmental arch window opening on the ground floor with a decorative buff brick voussoir and a wood lug sill.

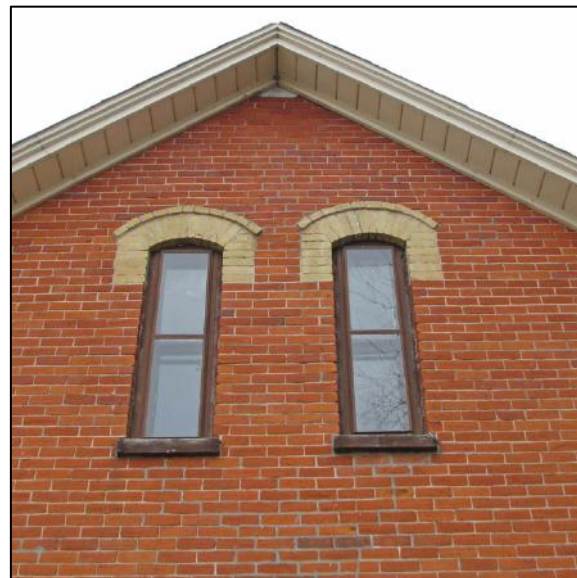


Figure 8. This view shows a typical pair of second floor, narrow, segmental arch, window openings accented with decorative buff brick voussoir and with wood lug sills.

The single ground floor window openings have a rough opening of 3-ft. 1½ in. (0.95 m) wide and 5-ft. 10-in. (1.78 m) high with wood lug sills measuring 3-ft. 9-in. (1.14 m) long and 4-in. (101.6 mm) high (**Figure 7**). The centre window opening of the bay measures 3-ft 1-in. (0.94 m) in width and about 7-ft. (2.13 m) in height. The side openings are narrower and measure 1 ft. 11-in. (0.58 m) in width. The period wood sash remains in all of the original window openings. The wood sash has a segmental head and 2/2 glazing pattern. The paired narrow windows openings have a 1/1 glazing pattern.

4.1.1 Main Section

Front (East) Elevation

The east elevation of the main section is divided into two bays with an entrance door and a window opening on the north end on the ground floor. There is a centre gable with a single window opening on the second floor. The window has decorative buff brick accent above and vergeboard. The vergeboard appears to be a 20th century replacement.

The main entrance door on the east elevation is set slightly off-centre to the south and has a segmental head and a wood sill. The opening measures 3-ft. 2-in. (0.97 m) wide and 7-ft. 3-in. (2.21 m) high to the centre of the arch. The door has upper glazing and two fielded panels below.

A full width open verandah spans the front elevation. It is a 20th century addition set on a concrete deck that probably replaced an earlier wood verandah. A wood balustrade encloses the north and east sides of the verandah. The deck is accessed by a set of stone steps on the south end. Wood posts that support the bellcast roof are distinguished by decorative fretwork. Ghost marks on the brick at the roofline on the north and south ends mark a previous verandah.

North elevation

The north elevation of the main house comprises a gable end with a single window opening centred in the wall of the ground floor and a pair of narrow window openings at the second floor level. A basement window opening with a red brick voussoir is situated under the ground floor opening.

West Elevation

The one and-a-half storey brick wing obscures most of the west wall of the main building.

South Elevation

The south elevation has a gable end wall with a one storey, ground floor, three sided bay window projection distinguished by a mansard roofline and decorative fascia board with

brackets. Each side of the bay has a single window opening. There is a basement window opening under the middle window with a buff brick voussoir. The second floor has a pair of narrow window openings over the bay window.

4.1.2 Rear Wing and Modern Addition

An original one and-a-half storey brick rear wing extends to the west of the main section.

The north elevation of the rear wing contains a single ground floor window opening centered in the wall. A former basement window with a red brick voussoir is located under the window.

The west elevation is a gable end. The second floor has a pair of narrow window openings similar to the other elevations. The gable peak once had a brick chimney perched on the roof ridge. The ground floor is obscured by a one storey, 20th century addition comprising a garage with two window openings.

The south elevation is obscured on the ground floor by the one storey addition. The west end is the garage entrance and the east part comprises an enclosed interior space, possibly a former side verandah area. The second floor has a centred gable with a single window opening.

4.2 Residence: Interior

The basement under the main house, the rear wing and the interior of the garage were not viewed.

The main entry on the east elevation leads directly into the south room on the ground. The floor plan of the main section comprises two rooms, the larger room on the south side with the bay window and a kitchen, and the smaller room on the north side. The north room is accessed through an east door opening. It appears to have been subdivided to provide a washroom accessible on the west wall. An enclosed flight of stairs to the second floor runs east to west between the two rooms. Examples of the original baseboard and wood trim with decorative box corners around some door and window openings are still in-situ.

A centred door opening on the west end of the south room in the main section leads to a large room on the ground floor of the rear wing. The entrance to the staircase to the second floor is located on the east wall. An adjacent door to its north provides access to the washroom located in the main section. Another door on the south wall leads to the 20th century addition space.

The staircase to the second floor leads up to a centre hall plan. On the second floor, it has a newel post and a railing comprising a handrail and turned balusters. Every second

baluster has been removed. Original baseboard, box corner door trim and floorboards are in-situ on the second floor.

Door openings at the top of the stairs provide access to a large bedroom on the south side and a small bedroom in the northeast corner. A bathroom is located in the northwest corner and accessed by a door on the north wall at the west end of the hall. The bathroom space appears to have been part of the northeast bedroom originally.

A door opening on the west end of the centre hall provides access to a large room on the second floor of the rear wing. The floor level is the same as the main section of the house. The room in the wing has a sloped roof on the north and south sides, a window opening on the south side in the gable and a pair of narrow window openings on the west wall.

4.3 Modifications

The subject residence has undergone some modifications, in particular, the one storey addition with a garage on the ground floor of the west and south elevations. The front verandah is also a 20th century replacement structure. Photographs of the house (2005) indicate the brick chimney on the west end of the roof ridge of the rear wing has been removed. There has been some deterioration of the building fabric over the years due to a lack of maintenance. There is water damage on the second floor due to a leaking roof.

Despite these alteration the building retains its original form and design character.

5.0 HERITAGE EVALUATION

5.1 Introduction

Criteria for determining cultural heritage value or interest are set out under Ontario Regulation 9/06 made under the Ontario Heritage Act (OHA), as amended in 2005. These criteria were developed to assist municipalities in the evaluation of properties considered for designation.

The Ontario Regulation 9/06 states that:

“A property may be designated under section 29 of the Act if it meets one or more of the following criteria for determining whether it is of cultural heritage value or interest:

- 1. The property has design value or physical value because it,*
 - i. is a rare, unique, representative or early example of a style, type, expression, material or construction method,*
 - ii. displays a high degree of craftsmanship or artistic merit,**or*
 - iii. demonstrates a high degree of technical or scientific achievement.*
- 2. The property has historical value or associative value because it,*

- i. has direct associations with a theme, event, belief, person, activity, organization or institution that is significant to a community,*
 - ii. yields, or has the potential to yield , information that contributes to an understanding of a community or culture, or*
 - iii. demonstrates or reflects the work or ideas of an architect, artist, builder, designer or theorist who is significant to a community.*
3. *The property has contextual value because it,*
- i. is important in defining, maintaining, or supporting the character of an area,*
 - ii. is physically, functionally, visually or historically linked to its surroundings,*
 - or*
 - iii. is a landmark.”*

Consultation with the City of Brampton indicates 10020 Mississauga Road is included on the City’s *Municipal Register of Cultural Heritage Resources* (April 2016). It is not municipally designated under the OHA. The site is not recognized through a local, provincial or federal plaque programme.

5.2 Evaluation

The evaluation criteria set out under Ontario Regulation 9/06 were applied to the residence at 10020 Mississauga Road.

5.2.1 Design Value or Physical Value

Design or Physical Value	
i. Rare, unique, representative or early example of a style, type, expression, material or construction method.	Yes
ii. Displays a high degree of craftsmanship or artistic merit.	Yes
iii. Demonstrates a high degree of technical or scientific achievement	No

i. Rare, unique, representative or early example of a style, type, expression, material or construction method.

Believed to have been built in the early 1880s, this vernacular brick residence exhibits design elements of the Gothic Revival architectural style such as a cross gable roof, centre gables with a window opening and vergeboard, a one storey bay window with decorative brackets and dichromatic buff brick accents over the door and window openings. The residence has undergone some alterations but is considered to retain its overall form and design character. The state of preservation of the interior of the building is fair.

Therefore, it is concluded the vernacular brick residence located at 10020 Mississauga Road, which was built in the early 1880s, is a representative example of brick construction that displays design aspects of the Gothic Revival style of architecture.

ii. Displays a high degree of craftsmanship or artistic merit.

The longevity of the building located at 10020 Mississauga Road attests to the quality of the craftsmanship and the building materials. The builder showed skill in the application of buff brick accent over the window and door openings and the use of paired narrow window openings on the second floor and the bay window detailing.

iii. Demonstrates a high degree of technical or scientific achievement

No aspects of technical or scientific merit were identified for the building located at 10020 Mississauga Road.

5.2.2 Historical Value or Associative Value

Historical or Associative Value	
i. Has direct associations with a theme, event, belief, person, activity, organization or institution that is significant to a community	No
ii. Yields, or has the potential to yield, information that contributes to an understanding of a community or culture	No
iii. Demonstrates or reflects the work or ideas of an architect, artist, builder, designer or theorist who is significant to a community	No

i. Has direct associations with a theme, event, belief, person, activity, organization or institution that is significant to a community

The brick residence located at 10020 Mississauga Road is not considered to have a direct association with a theme that is significant to the community.

ii. Yields, or has the potential to yield, information that contributes to an understanding of a community or culture

The brick residence located at 10020 Mississauga Road is not considered to have the potential to yield information on the understanding of a community or culture.

iii. Demonstrates or reflects the work or ideas of an architect, artist, builder, designer or theorist who is significant to a community

The identity of the builder/designer of the residence at 10020 Mississauga Road has not been identified.

5.2.3 Contextual Value

Contextual Value	
i. Is important in defining, maintaining, or supporting the character of an area.	Yes
ii. Is physically, functionally, visually or historically linked to its surroundings.	Yes
iii. Is a landmark.	Yes

i. Is important in defining, maintaining, or supporting the character of an area.

Once located in a well-established agricultural landscape with scattered farmsteads, tree lines, hedgerows, fence lines that defined the area, the brick residence at 10020 Mississauga Road now sits within a commercial development setting that is defined by The Apple Factory business. The surrounding area is under development from rural agricultural to urban use. With other farmhouses on the west side of Mississauga Road to be preserved, this brick residence maintains a visual and historical link to the former character of the area.

Therefore the residence at 10020 Mississauga Road is considered to be important in maintaining the a connection to the former agricultural character of an area.

ii. Is physically, functionally, visually or historically linked to its surroundings.

The residence at 10020 Mississauga Road was built in the early 1880s. It remains on its original site on a corner lot facing eastward to Mississauga Road, which developed as an important historic transportation route in Chinguacousy Township. With other farmhouses to be preserved within the urban development of the west side of Mississauga Road, this brick residence maintains a visual historical link to the former rural agricultural character of the area. It no longer serves its original function as a rural residence.

It is concluded the brick residence is physically, visually and historically linked to its surroundings.

iii. Is a landmark.

Located on a corner lot, the brick residence located at 10020 Mississauga Road is considered to be familiar structure to nearby residents and travellers due to its longevity, its distinctive design elements that identify it as being of 19th century construction, the views to the residence from Mississauga Road and Bovaird Drive, and its association with The Apple Factory, which has been a very popular commercial store in the area since the late 20th century.

Therefore, the brick residence is considered to be a local landmark.

5.3 Summary of Heritage Evaluation Results

It is determined through the application of the “Criteria for Determining Cultural Heritage Value or Interest” under Ontario Regulation Reg. 9/06 that the subject property at 10020 Mississauga Road is of cultural heritage value or interest, due to its physical or design value and contextual value.

5.3.1 Statement of Cultural Heritage Value

The brick residence at 10020 Mississauga Road seems to have been built in the early 1880s as a residence for James Hunter Scott and Mary Amanda Scott. The Scott family owned the property for thirty years from 1881 to 1911.

The house is a representative example of brick construction that displays design aspects of the Gothic Revival style of architecture. Characteristic Gothic Revival elements include a cross gable roof, centre gables with a single window opening and decorative vergeboard, a one storey bay window with decorative brackets and dichromatic buff brick accents over the door and window openings. Although the residence has undergone some alterations, it is considered to have retained its overall form and design character and is a representative example of brick construction of longevity (about 135 years) and craftsmanship that displays design aspects of the Gothic Revival style of architecture.

Once located in a well-established agricultural landscape with scattered farmsteads, tree lines, hedgerows, fence lines that defined the area, the brick residence now sits within a commercial development setting characterized by The Apple Factory commercial development. With other farmhouses to be preserved within the urban development of the west side of Mississauga Road, this brick residence maintains a visual and historical link to the former rural agricultural character of the area. The residence remains on its original site on a corner lot facing eastward to Mississauga Road, which became, and still is, an important, transportation route in Chinguacousy Township. The brick residence is considered to be physically, visually and historically linked to its surroundings.

Located on a corner lot, the brick residence located at 10020 Mississauga Road is considered to be familiar landmark structure to nearby residents and travellers due to its longevity, its distinctive design elements that identify it as being of 19th century construction, the views to the residence from Mississauga Road and Bovaird Drive, and its association with The Apple Factory, which has been a very popular commercial store in the area since the late 20th century.

5.3.2 Description of Heritage Attributes

Heritage attributes, i.e., character defining elements, of the subject property at 10020 Mississauga Road include, but are not limited to the following details.

Site

- The site on the west side of Mississauga Road in the northwest corner of the intersection of Mississauga Road and Bovaird Drive West, comprising the East Part of Lot 11, Concession 5 WHS of the geographic Township of Chinguacousy.
- The orientation of the brick residence that faces east to Mississauga Road.

Residence Exterior

- 'T'-shape plan with one and-a-half-storey main section and one and-a-half-storey rear wing composition.
- Cross gable roof with centre front (east) gable and centre (south) gable on the rear wing.
- Rubblestone foundation and brick wall construction with a running bond pattern.
- Segmental door and window openings on the ground and second floor elevations accented with dichromatic buff brick detailing and the wood lug sills.
- One storey bay window on the south elevation of the main house with mansard roof and decorative brackets and wood details.
- Pairs of narrow window openings on the second floor of the south and north elevations of the main house and the west elevation of the rear wing.

Residence Exterior

- Interior floor plan
- Original door and window trim and original baseboard

6.0 THE DEVELOPMENT PROPOSAL

The Region of Peel proposes to widen and improve Mississauga Road. It has undertaken EA studies for road improvements in the area that include the intersection of Bovaird Drive West and Mississauga Road.

R.V. Anderson Associates Limited provided the following design drawings for future road improvements to the intersection of Mississauga Road and Bovaird Drive West for the following EA studies:

- Mississauga Road From Williams Parkway to Bovaird Drive that shows four lanes of traffic to be completed in 2017; and
- Mississauga Road from Bovaird Drive West to Mayfield Road that shows six lanes of traffic to be tentatively completed for 2020.

7.0 SUMMARY OF RELEVANT HERITAGE POLICIES

7.1 The Planning Act and Provincial Policy Statement (2014)

Section 2 of the *Planning Act* identifies “*matters of provincial interest, which includes the conservation of significant features of architectural, cultural, historical, archaeological or scientific interest*” (Heritage Resources in the Land Use Planning Process).

Section 3 of the *Planning Act* enables the Province to issue Policy Statements on matters of Provincial Interest. The Provincial Policy Statement (PPS) (2014) issued under the

Planning Act applies to this proposal. PPS Section 2.6 addresses Cultural Heritage and Archaeology. Policy 2.6.1 of the PPS states:

“Significant built heritage resources and significant cultural heritage landscapes shall be conserved.”

The PPS provides the following definitions to the words shown in bold italics above.

Significant: means in regard to cultural heritage and archaeology, “... resources that have been determined to have cultural heritage value of interest for the important contribution they make to our understanding of the history of a place, an event, or a people.”

Built heritage resource: means a building, structure, monument, installation or any manufactured remnant that contributes to a property’s cultural heritage value or interest as identified by a community, including an Aboriginal community. Built heritage are generally located on property that has been designated under Parts IV or V of the OHA, or included on local, provincial and/or federal registers.

Cultural heritage landscape: means a defined geographical area that may have been modified by human activity and is identified as having cultural heritage value or interest by a community, including an Aboriginal community. The area may involve features such as structures, spaces, archaeological sites or natural elements that are valued together for their interrelationship, meaning or association. Examples may include, but are not limited to, heritage conservation districts designated under the OHA; villages, parks, gardens, battlefields, mainstreets and neighbourhoods, cemeteries, trailways, viewsheds, natural areas and industrial complexes of heritage significance; and areas recognized by federal or international designation authorities (e.g. a National Historic Site or District designation, or a UNESCO World Heritage Site).

Conserved: means the identification, protection, management and use of built heritage resources, cultural heritage landscapes and archaeological resources in a manner that ensures their heritage value or interest is retained under the OHA. This may be achieved by the implementation of recommendations set out in a conservation plan, archaeological assessment and/or heritage impact assessment. Mitigation measures and/or alternative development approaches can be included in these plans and assessments.”

The PPS also deals with development adjacent to a protected heritage property in Policy 2.6.3. It states,

“Planning authorities shall not permit **development** and **site alteration** on **adjacent lands** to **protected heritage property** except where the proposed **development** and **site alteration** has been evaluated and it has been demonstrated that the **heritage attributes** of the **protected heritage property** will be **conserved**.”

The PPS provides the following definitions to the words shown in bold italics above.

Development: means the creation of a new lot, a change in land use, or the construction of buildings and structures, requiring approval under the *Planning Act*.

Site alteration means activities, such as grading, excavation and the placement of fill that would change the landform and natural vegetative characteristics of a site.

Adjacent lands: means, "...for the purposes of policy 2.6.3, those lands contiguous to a *protected heritage* property or as otherwise defined in the municipal official plan".

Protected heritage property: means property designated under Part IV, V or VI of the *Ontario Heritage Act*; property subject to a heritage conservation easement under Parts II or IV of the *Ontario Heritage Act*; property identified by the Province and prescribed public bodies as provincial heritage property under the Standards and Guidelines for Conservation of Provincial Heritage Properties; property protected under federal legislation, and UNESCO World Heritage Sites.

Heritage attributes: means the principal features or elements that contribute to a *protected heritage property's* cultural heritage value or interest, and many included the property's built or manufactured elements, as well as natural landforms, vegetation, water features, and its visual setting (including significant views or vistas to or from a *protected heritage property*).

Therefore, in adjacency situations "heritage attributes" of heritage properties are to be interpreted broadly and not necessarily limited to those listed in a designation by-law. The visual relationship between the old and new is a key consideration. The new work should not visually overwhelm, diminish or degrade the old and create an adverse effect to the heritage attributes.

Conserved is defined above.

7.2 Ontario Heritage Act (OHA)

Part IV of the OHA enables municipalities to list and to designate properties of cultural value or interest after consultation with its heritage advisory committee, if one is appointed. Section 27 of the Act requires the clerk of every municipality to keep a register of properties of cultural heritage value or interest in the municipality. As of 2005, subsection 27.1 of the Act allows municipal councils to include on the municipal register properties of cultural heritage value that have not been designated under the OHA (listed properties) after the council has consulted with its municipal heritage advisory committee.

The Provincial Government has established criteria for determining the cultural heritage value or interest of properties through Regulation 9/06 (*see Section 5.0*). Once a property

is designated under the OHA, it may not be altered or demolished without the approval of the municipal council. An owner may appeal Council's decision on an application to alter or demolish to the Ontario Municipal Board.

Once a property is listed in the municipal register under the OHA, any application to demolish a building on a listed property is delayed for 60 days from the date when Council is notified of the proposed demolition, during which Council may pursue designation of the property.

The OHA also enables municipalities to enter into easement agreements for the conservation of property of cultural heritage value or interest (section 37). Such easements run with the title to the property and municipalities may enforce such easements. The decision of a municipal council with respect to an easement is final.

7.3 City of Brampton Official Plan

The requirement to provide a HIA is included in Section 4.9 of the City of Brampton's Official Plan (OP). According to Section 4.9.1.10 of the OP,

A Heritage Impact Assessment, prepared by a qualified heritage conservation professional, shall be required for any proposed alteration, construction, or development involving or adjacent to a designated heritage resource to demonstrate that the heritage property and its heritage attributes are not adversely affected. Mitigation measures and/or alternative development approaches shall be required as part of the approval conditions to ameliorate any potential adverse impacts that may be caused to the designated heritage resources and their heritage attributes.

Official Plan Policy 4.9.1.11 states,

A Heritage Impact Assessment may also be required for any proposed alteration work or development activities involving or adjacent to heritage resources to ensure that there will be no adverse impacts caused to the resources and their heritage attributes. Mitigation measures shall be imposed as a condition of approval of such applications.

Official Plan Policy 4.9.1.12 outlines and prioritizes preferred mitigation options starting with onsite retention.

In addition, Official Plan Implementation Policy 4.9.9.2 (ii) allows for:

Requiring the preparation of a Heritage Impact Assessment for development proposals and other land use planning proposals that may potentially affect a designated or significant heritage resource or Heritage Conservation District.

The Planning, Design and Development, Heritage, of the City of Brampton prepared guidelines, *Heritage Impacts Assessment –Terms of Reference*, for the preparation of an HIA. It states an HIA will be required for the following:

- Any property listed or designated in the municipal heritage register, pursuant to Section 27 (1.1) or (1.2) of the *Ontario Heritage Act* that is subject to land use planning applications;
- Any property listed or designated in the municipal heritage register, pursuant to Section 27 (1.1) or (1.2) of the *Ontario Heritage Act* that is facing possible demolition; and
- Any property that is subject to land use planning applications and is adjacent to a property designated in the municipal heritage register, pursuant to Section 27 (1.1) of the *Ontario Heritage Act*.

A HIA may be required for the following:

- Any property that is subject to land use planning applications and is adjacent to a property listed in the municipal heritage register, pursuant to Section 27 (1.2) of the *Ontario Heritage Act*.

As well a property does not have to be designated or listed in a heritage register to be subject to a Heritage Impact Assessment. Any property that may exhibit cultural heritage value or interest or ‘heritage potential’ as determined by City heritage staff will be subject to an appropriate level of heritage due diligence and may require an HIA. An HIA may be ‘scoped’ based on the specific circumstances and characteristics that apply to a heritage resource. Consultation with City heritage staff will be required to determine when a scoped HIA may be required, as well as requirements for the content.

7.4 Municipal Heritage Status of Adjacent Properties

The following properties are included on its *Municipal Register of Cultural Heritage Resources* (April 2016) or the *Municipal Register of Cultural Heritage Resources Designated Under the Ontario Heritage Act* (April 2016). As well, the City of Brampton has identified the following properties as being in the vicinity of the subject property on an aerial view included in **Appendix D**.

- J.N.O. McClure Farm, 10244 Mississauga Road, “listed” property on the City Heritage Inventory;
- 11248 Mississauga Road, a “listed” property on the City Heritage Inventory;
- Dolson Farm, 11722 Mississauga Road, municipally designated under the *Ontario Heritage Act* (2013); and
- 1985 Bovaird Drive West, McCandless Plank House, Municipal Designation under the OHA pending.

8.0 ASSESSMENT OF THE IMPACT OF THE PROPOSED DEVELOPMENT

This section provides an assessment of the potential adverse effects of the proposed widening of Mississauga Road at 10020 Mississauga Road. The property at 10020 Mississauga Road is listed in the City of Brampton's *Municipal Register of Cultural Heritage Resources* (April 2016).

The conservation of cultural heritage resources in planning is considered to be a matter of public interest and of municipal and/or regional government. Generally, road improvement projects such as the widening of an existing roadway, revisions to existing intersections and grading have the potential to adversely affect cultural heritage landscapes and built heritage resources by displacement and/or disruption during, as well as after construction. Cultural heritage landscapes and/or built heritage resources may experience displacement, or direct impacts, i.e., removal, if they are located within the right-of-way of the undertaking. There may also be potential for disruption, or indirect impacts, to cultural heritage resources by the introduction of physical, visual, audible or atmospheric elements to the existing heritage environment that are not in keeping with their character and/or setting. Negative impacts, as outlined in the *Ontario Heritage Tool Kit* may include, but are not limited to:

- Destruction of any, or part of any, significant heritage attributes or features;
- Alteration that is not sympathetic, or is incompatible, with the historic fabric and appearance;
- Shadows created that alter the appearance of a heritage attribute or change the viability of an associated natural feature or plantings, such as a garden;
- Isolation of a heritage attribute from its surrounding environment, context or a significant relationship;
- Direct or indirect obstruction of significant views or vistas within, from, or of built and natural features;
- A change in land use (such as rezoning a church to a multi-unit residence) where the change in use negates the property's cultural heritage value; and
- Land disturbances such as a change in grade that alters soils, and drainage patterns that adversely affect a cultural heritage resource, including archaeological resources.

R.V. Anderson Associates Limited provided the following design drawings for future road improvements to the intersection of Mississauga Road and Bovaird Drive West. The design drawings were reviewed to assess potential adverse impacts to the residence located at 10020 Mississauga Road. The Region of Peel has indicated the residence on the property is not required for road widening purposes in 2017; however, road widening will affect the frontage of the property.

EA Study Mississauga Road From Williams Parkway to Bovaird Drive

The Mississauga Road (From Williams Parkway to Bovaird Drive) Grading, Pavement & Drainage, Project 06-4025, Plan No.: NC-5, November 2015, prepared by The Municipal Infrastructure Group Ltd., MMM Group Limited and the Region of Peel in November 2015 was reviewed (**Figure 9**). The improvements are to be completed in 2017 include grading requirements on the west side of Mississauga Road that includes the frontage at the subject brick residence at 10020 Mississauga Road. The primary impacts of the proposed road improvements to the “listed” heritage property on the City of Brampton *Municipal Register of Cultural Heritage Resources* will be the reduction of the property frontage on Mississauga Road resulting in a smaller buffer zone between the brick residence and vehicular traffic and a change in the existing character and setting of the brick residence.

EA Study Mississauga Road from Bovaird Drive West to Mayfield Road

The Plan & Profile, Bovaird Drive Class Environmental Assessment Region of Peel prepared by AMEC in June 2012 for the grading, paving and drainage improvements to the intersection of Bovaird Drive West and Mississauga Road was reviewed (**Figure 10**). The primary impacts of the proposed road improvements and widening of Mississauga Road to six lanes of traffic by 2027 will be the acquisition of property on the west side of Mississauga Road that will result in the demolition/removal of the existing brick residence at 10020 Mississauga Road, currently a “listed” heritage property on the City of Brampton *Municipal Register of Cultural Heritage Resources*.

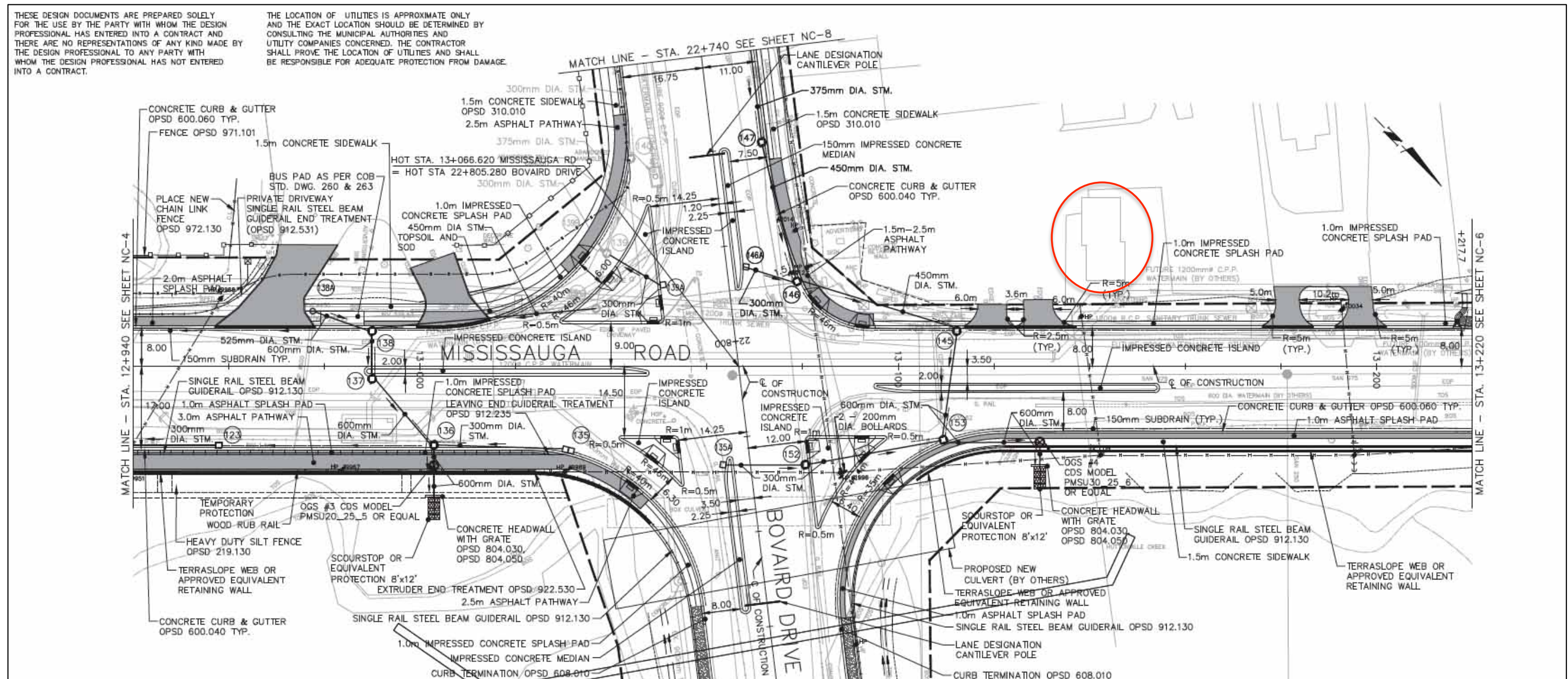
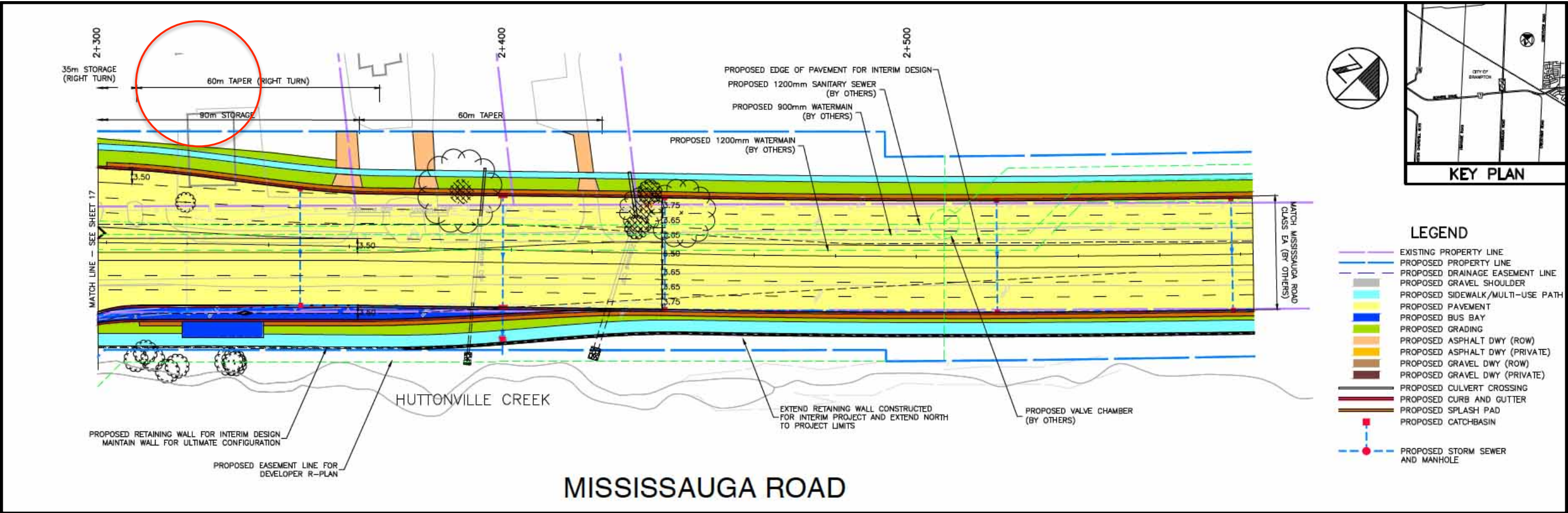


Figure 9. 2017 Improvements at intersection of Bovaird Drive and Mississauga Road. The circle marks the location of the subject residence at 10020 Mississauga Road [Mississauga Road and Bovaird Drive West, City of Brampton [Issued for Tender, November 2015, Mississauga Road (From Williams Parkway to Bovaird Drive) Grading, Pavement & Drainage, Project 06-4025, Plan No.: NC-5, November 2015, The Municipal Infrastructure Group Ltd., MMM Group and Region of Peel].



9.0 CONSERVATION AND MITIGATION RECOMMENDATIONS

9.1 Introduction

A proposed development should not adversely affect cultural heritage resources and intervention should be managed in such a way that its impact is sympathetic with the value of the resources and that it minimizes or avoids an adverse effect to a cultural heritage resource. When the nature of the undertaking is such that adverse impacts are unavoidable it may be necessary to implement management or mitigation strategies that alleviate the deleterious effects to cultural heritage resources. Mitigation measures lessen or negate anticipated adverse impacts to cultural heritage resources. These measures may include such actions as avoidance, monitoring, protection, relocation documentation, salvage, remedial landscaping, etc., and may be a temporary or permanent action.

The principal heritage philosophy for the protection of cultural heritage resources is retention *in situ*. The protection of built heritage resources is to preserve *in situ* the structures and their material integrity to the maximum extent possible, consistent with public safety. The following heritage conservation options, listed in descending order of preference, should be considered within the context of the project,

- Retention of the existing built heritage resource *in situ*.
- Relocation of the existing built heritage resource in a new location on its current site.
- Relocation of the existing built heritage resource to an appropriate new site nearby in its municipality, preferably in the vicinity of the existing site to preserve its historical value.
- Salvage of elements of the built heritage resource for incorporation into other structures.
- Full recording and documentation of the built heritage resource and its associated cultural heritage landscape if the structure is to be demolished.

When the nature of the alterations to a heritage site is such that adverse impacts are unavoidable, it may be necessary to implement the management of mitigation strategies that alleviate the deleterious effects to the cultural heritage resource. Mitigation strategies identified within the *Ontario Heritage Tool Kit* may include, but are not limited to,

- alternative development approaches;
- isolating development and site alteration from significant built and natural features and vistas;
- design guideline that harmonize mass, setback, setting and materials;
- limiting height and density;
- allowing only compatible infill and additions; and
- reversible alterations.

Mitigation measures and best management practices will be implemented to address potential impacts. Identified mitigation strategies will be carried. Refinements and enhancements to the mitigation recommendations will be made as warranted throughout all phases of the project.

9.2 Conservation and Mitigation Recommendations

The cultural heritage resource, namely the early 1880s brick residence located at 10020 Mississauga Road, is considered to be of cultural heritage value or interest to the City of Brampton. It is currently “listed” on the City of Brampton’s *Municipal Register of Cultural Heritage Resources* as a resource of heritage value. It has been evaluated under Ontario Regulation 9/06 and it is concluded it is worthy of consideration for municipal designated under Part IV of the OHA (*See Section 5.3.1 Statement of Cultural Heritage Value and 5.3.2 Heritage Attributes*).

As a “listed” heritage resource that has been shown to meet one or more of the evaluation criteria set out under Ontario Regulation 9/06, it is desirable to retain the early 1880s brick residence located at 10020 Mississauga Road on its original site with a suitable amount of frontage to buffer it from increased vehicular traffic on Mississauga Road.

In order to alleviate the adverse effects of the proposed road widening and improvements the following mitigation and conservation strategies are recommended.

2017

- The City of Brampton consider proceeding with the designation of the listed property located at 10020 Mississauga Road under Part IV of the OHA.
- There should be vibration monitoring of the brick residence during construction for the widening of Mississauga Road to four lanes.
- At a minimum the subject residence at 10020 Mississauga Road should be left with suitable frontage on Mississauga Road to provide a buffer from vehicular traffic. The Region of Peel should provide appropriate landscaping after construction activities cease in 2017 to rehabilitate the frontage and to provide a buffer zone from road traffic.

2020 and Beyond

- For the long term, if the currently municipally listed, and in the future, potentially municipally designated heritage resource, ie., the brick residence located on the subject site, is to be adversely affected by any future projects, the proponent shall work with the City of Brampton to prepare a plan to mitigate impacts.

City of Brampton

- If the City of Brampton proceeds with and approves the municipal designation of the subject property under the OHA, a Strategic Conservation Plan (SCP) should be prepared in cooperation with the property owner. The SCP will address

mitigation actions to be implemented to ameliorate long term effects of the potential for future demolition/removal or relocation of the brick residence at 10020 Mississauga Road.

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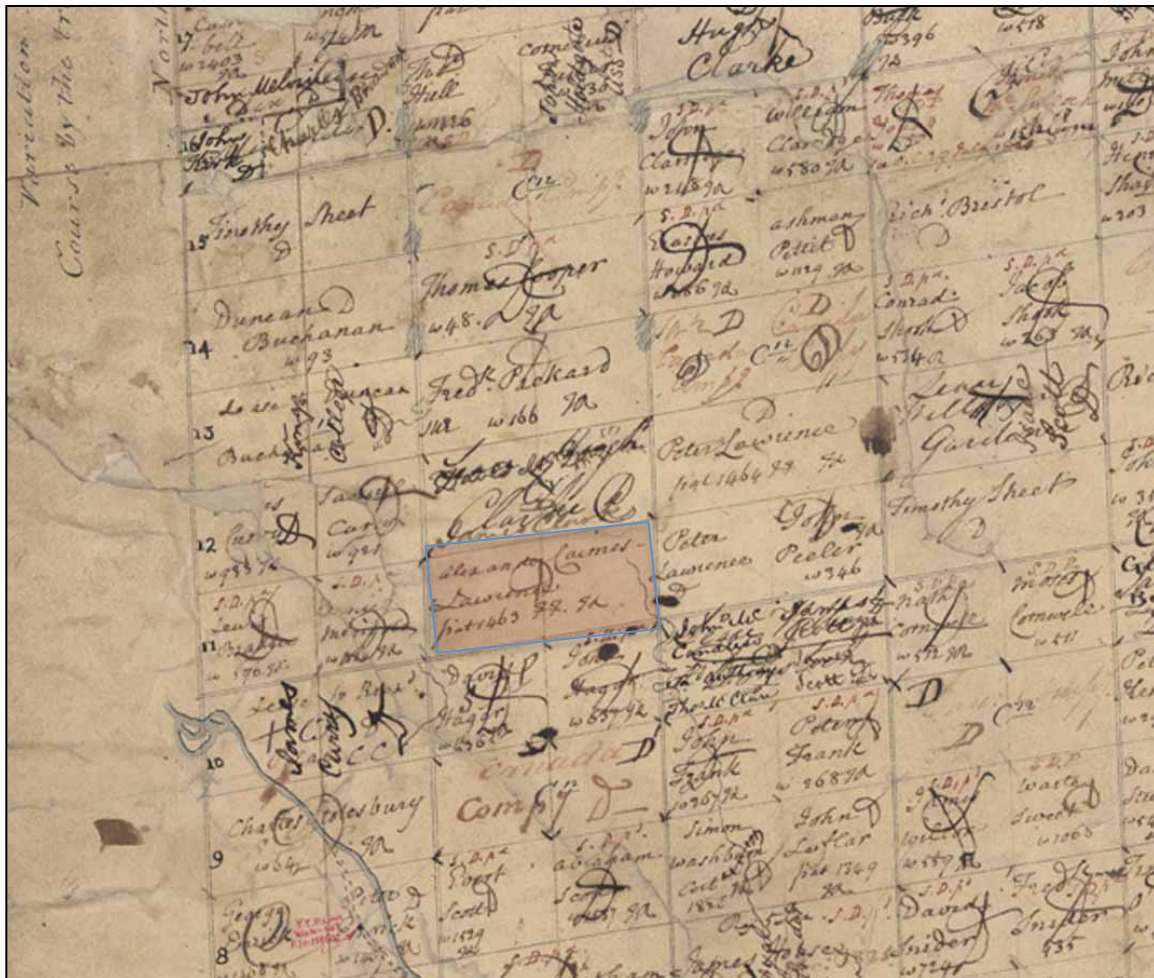
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Contact

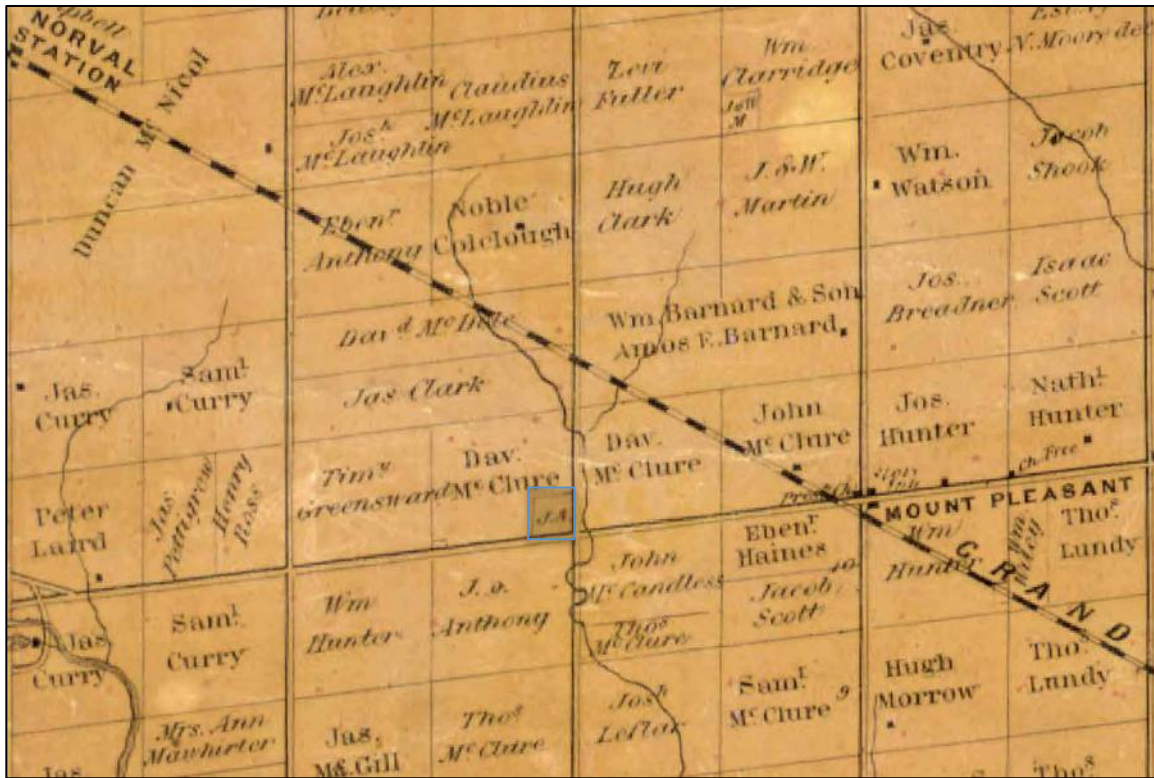
Antonietta Minichillo, Heritage Coordinator, City of Brampton.

James Hunt, Property Owner.

APPENDIX A:
Historical Maps and
Photographs



Alexander Cairns Lawrence is shown as the owner of a 200-acre Lot 11, Concession 5 WHS, Chinguacousy Township as marked on the map [Item #: RG 1-100-0-0-1906, Digital # 10044609, Maps Crown Lands, Part Town of Caledon – Part City of Brampton – Chinguacousy. Original Chinguacousy Township (Map #51), T. Ridout, Survey General, 1822; County of Peel October 1851, Chinguacousy}.



The highlighted area shows the location of the subject property at 10020 Mississauga Road in the mid 19th century [Tremaine's Map of the County of Peel, Canada West. Toronto: C.R. & C.M. Tremaine, 1859].



The highlighted area shows the location of the subject property at 10020 Mississauga Road in Chinguacousy Township [Illustrated Historical Atlas of the County of Peel. Toronto: Walker & Miles, 1877].



The circle shows the location of the brick house on the subject property at 10020 Mississauga Road NTS Brampton Sheet 35, survey 1907, revised 1909].



The circle shows the location of the brick house and a barn structure on the subject property at 10020 Mississauga Road NTS Brampton 30 M 12, survey 1907, revised 1942].



This aerial view shows the rural agricultural character of the landscape along Mississauga Road north of Highway 7 (Bovaird Drive West) in the early 1950s. The circle highlights the subject property at 10020 Mississauga Road. Note the residences at 10042 and 10054 Mississauga Road are not shown and a barn appears to be located to the west of the house [UofT, Digital Aerial Photographs. Southern Ontario 1954, Hunting Survey Corporation Limited, #436.794].



This view shows the front (east) elevation of the residence located at 10020 Mississauga Road (UMcA, 2005).



This view shows the south elevation of the residence located at 10020 Mississauga Road (UMcA, 2005).



This view shows the west elevation of the residence located at 10020 Mississauga Road. Note the brick chimney, which has been removed (UMcA, 2005).

APPENDIX B:
Photographs
Cultural Heritage Landscape



View west on Bovaird Drive West to Mississauga Road with the residence at 10020 Mississauga Road on the right. Seasonally this view may be partially obscured by vegetation.



The Apple Factory market building with the brick residence in the background on the right.



View northeast across The Apple Factory parking lot to the brick residence.



A view north along Mississauga Road depicts the two-lane paved roadway and the brick residence at 10020 Mississauga Road set close to the roadway.



View south across paved entrance access to the site on the north side of the brick residence.



The two residences located on the north side of the brick residence at 10042 (left) and 10054 (right) Mississauga Road. Note the Canada Post community box in front of the house at 10042.

APPENDIX C:
Photographs
Built Heritage Resources



The building is a vernacular styled residence with Gothic Revival detailing with a centre gable with decorative vergeboard and dichromatic detailing over the window and door openings.



The south elevation is highlighted by a ground floor bay window and a pair of narrow window openings on the second floor.



This view to the northeast shows the rear wing and the one storey addition including a garage entrance on the south side.



This view shows the west elevation.



This is a view of the northwest corner of the residence.



The north elevation shows the main section and the inset rear wing on the west side.



This view shows the northeast corner of the residence.



This view is of the exterior of the front (east) entrance door.



This view shows the centre gable front (east) elevation.



The circle indicates the location of markings brickwork on the north end of the verandah indicating an earlier verandah.



The circle indicates the location of markings brickwork on the south end of the verandah indicating an earlier verandah.



View of the interior trim of the front entrance door.



Door on the west wall of south room on ground floor.



This view shows the bay window openings on the south wall of the south ground floor room of the main section.



This view shows the pair of narrow window openings and trim on the west wall of the second floor room of the rear wing.



This view looks west along the second floor hall to a door opening on the west wall that leads to the rear wing. The newel post and balustrade of the staircase are also shown.



This view looks east in the second floor centre hall to the front gable window openings. The two bedrooms doors are shown on either side of the hall. The centre gable window is on the east wall the top of the stairs.

APPENDIX D:
Map of Adjacent
Heritage Properties



Map showing the location of the subject property at 10020 Mississauga Road and adjacent heritage properties [Provided by City of Brampton, 2016].