Flower City



For Office Use Only (to be inserted by the Secretary-Treasurer after application is deemed complete)

FILE NUMBER: A- 2025-00

The Personal Information collected on this form is collected pursuant to section 45 of the Planning Act and will be used in the processing of this application. Applicants are advised that the Committee of Adjustment is a public process and the information contained in the Committee of Adjustment files is considered public information and is available to anyone upon request and will be published on the City's website. Questions about the collection of personal information should be directed to the Secretary-Treasurer, Committee of Adjustment, City of Brampton.

APPLICATION Minor Variance or Special Permission

(Please read Instructions)

NOTE: It is required that this application be filed with the Secretary-Treasurer of the Committee of Adjustment and be accompanied by the applicable fee. The undersigned hereby applies to the Committee of Adjustment for the City of Brampton under section 45 of the Planning Act, 1990, for relief as described in this application from By-Law 270-2004. Name of Owner(s) Williams Parkway Torbram Holdings LP Address 30 Adelaide Street East, Suite 301 Toronto, ON M5C 3H1 Phone # Fax # 416-365-3535 mains Athebiglierigroup.com **Email** mmirandala Mallory Nievas c/o The Biglieri Group 2. Name of Agent

3. Nature and extent of relief applied for (variances requested):

North Park Drive Setback: Required - 25 m, Requested - 24.3 m

Parking Spaces: Required - 477, Requested - 381

Height: Required - 10.8 m, Requested - 14.31 m

Outdoor Storage setbacks: Required - Williams Parkway: 150m, Torbram Road: 90 m,

4. Why is it not possible to comply with the provisions of the by-law?

The zoning was originally designed to support auxiliary parking for the existing adjacent industrial use. Since the zoning was created, the land has undergone a consent to sever the land. To facilitate the proposed development updates to the existing zoning provisions are necessary, hence why relief is required.

are necessary, nonce why rener to require .

Lot Number PART LOTS 8 & 9

Plan Number/Concession Number

Municipal Address

9501-9511 Torbram Road, Brampton ON

6. Dimension of subject land (in metric units)

Frontage 665.81 m (approx.)

Depth Area 174 m (approx. 13.03 ha (approx.) 13.03 ha (approx.)

7. Access to the subject land is by:
Provincial Highway
Municipal Road Maintained All Year
Private Right-of-Way

Legal Description of the subject land:

5.

| Seasonal Road |
|-------------------|
| Other Public Road |
| Water |

| 8. | Particulars of all buildings and structures on or proposed for the subject land: (specify in metric units ground floor area, gross floor area, number of storeys, width, length, height, etc., where possible) | | |
|---------|--|---------------------------------------|--|
| | EXISTING BUILDING | SS/STRUCTURES on th | ne subject land: <u>List all structures (dwelling, shed, gazebo, etc.)</u> |
| | N/A | | |
| | PROPOSED BUILDI | NGS/STRUCTURES on | the subject land: |
| | The proposed dev | velopment include tw | vo one-storey industrial buildings. |
| 9. | | • | ructures on or proposed for the subject lands: and front lot lines in <u>metric units</u>) |
| | EXISTING | | |
| | Front yard setback | N/A | Wilder and the second s |
| | Rear yard setback | N/A | |
| | Side yard setback Side yard setback | N/A N/A | |
| | Side yard Selback | N/A | |
| | PROPOSED | | |
| | Front yard setback | 20.7 | |
| | Rear yard setback | 24.3 | |
| | Side yard setback | 24.2 (Torbram) | |
| | Side yard setback | 32.4 | |
| 10. | Date of Acquisition | of subject land: | December 2024 |
| 11. | Existing uses of sul | bject property: | Parking Lot |
| 12. | Proposed uses of s | ubject property: | Industrial_ |
| | | | |
| 13. | Existing uses of abo | utting properties: | Industrial - Automotive Assembly Plant |
| 14. | Date of construction | n of all buildings & stru | uctures on subject land: n/a |
| 15. | Length of time the existing uses of the subject property have been continued: n/a | | bject property have been continued: n/a |
| 16. (a) | What water supply Municipal Well | is existing/proposed? | Other (specify) |
| (b) | What sewage dispo Municipal Septic | osal is/will be provided ^a | ? Other (specify) |
| (c) | What storm drainag | ge system is existing/p | roposed? |
| | Ditches | <u> </u> | Other (specify) |

Swales

| 17. | Is the subject property the subject subdivision or consent? | ect of an application under the | he Planning Act, for approval of a plan of | of |
|---|---|--|--|--|
| | Yes 🗸 No 🗌 | | | |
| | If answer is yes, provide details: | File # B-2024-0006 | Status Approved | _ |
| 18. | Has a pre-consultation application | on been filed? | | |
| | Yes 🗸 No 🔲 | | | |
| 19. | Has the subject property ever be | en the subject of an applicat | ion for minor variance? | |
| | Yes 🗸 No 🔲 | Unknown | | |
| | If answer is yes, provide details: | | | |
| | File # A-2024-0136 Decision File # Decision | | Relief Setbacks and Landscaping Relief | |
| | File # Decision | | Relief | |
| | | | | |
| | | Signatura | of Applicant(s) or Authorized Agent | _ |
| DAT | TED AT THE COM | OF Signature | Applicant(s) of Additionized Agent | |
| TUI | S 2D DAY OF | 1/W 20 0 | | |
| i | APPLICATION IS SIGNED BY AN A | - C | DEDONI OTHER THAN THE OWNER O | - |
| THE SUE | BJECT LANDS, WRITTEN AUTHOR | IZATION OF THE OWNER MI | PERSON OTHER THAN THE OWNER O UST ACCOMPANY THE APPLICATION. I | F |
| | PLICANT IS A CORPORATION, RATION AND THE CORPORATION'S | | BE SIGNED BY AN OFFICER OF TH | E |
| | | 95 TUE | 25. | |
| | I, Mallory Nievas c/o The Biglieri Group | COMPANIENCE INC. CONTROL CONTR | City OF Toronto | _ |
| | E Province OF Ontario | SOLEMNLY DECL | | |
| | | | EMN DECLARATION CONSCIENTIOUSL FORCE AND EFFECT AS IF MADE UNDE | |
| DECLAR | ED BEFORE ME AT THE | | Emily Marie I | Hill, a Commissioner, |
| | OF | | etc., Province | of Ontario, for The |
| IN THE | CITY OF | | Wall An Ecol | Ltd. and The Living ogical Sound Barrier |
| 100.00 V | | | Solution Inc., I | Expires November 15, |
| TORON | NTOTHIS 02 DAY OF | | 077114 | 2027. |
| MP | <u>RIL</u> , 20 <u>AS</u> | Signatu | re of Applicant or Authorized Agent | |
| | 30000 | | | |
| *************************************** | A Commissioner etc. | | | |
| - | | FOR OFFICE USE ONLY | | ٦ |
| | Present Official Plan Designatio | n: | | |
| | Present Zoning By-law Classific | • | | |
| | Enforcement Action File Numbe | | | |
| | This application has been review said review | ed with respect to the variance w are outlined on the attached | es required and the results of the checklist. | |
| | | | | |
| | Zoning Officer | | Date | |
| | 5 A TE 55 C F B C C C B C C B C C B C C B C C B C C C B C C B C C C B C C C B C C C B C C C B C | Mail | 21 2025 | normal. |
| | DATE RECEIVED Date Application Deemed | | Revised 2022/02/17 | |
| | Complete by the Municipality | | MA/A. | |
| | | | | |

APPOINTMENT AND AUTHORIZATION OF AGENT

To: The Secretary-Treasurer
Committee of Adjustment
City of Brampton
2 Wellington Street West
Brampton, Ontario
L6Y 4R2
coa@brampton.ca

| LOCATION OF THE SUBJECT LAND: 9501-9511 Torbram Road | | | | |
|--|--|--|--|--|
| I/We, Williams Parkway Torbram Holding LP please print/type the full name of the owner(s) | | | | |
| please printrype the full flame of the owner(s) | | | | |
| the undersigned, being the registered owner(s) of the subject lands, hereby authorize | | | | |
| Mallory Nievas c/o The Biglieri Group | | | | |
| please print/type the full name of the agent(s) | | | | |
| to make application to the City of Brampton Committee of Adjustment in the matter of an application for minor variance with respect to the subject land. | | | | |
| Dated this 17 day of April , 20 25. | | | | |
| Bar San | | | | |
| (signature of the owner[s], or where the owner is a firm or corporation, the signature of an officer of the owner.) | | | | |
| Brad Keast | | | | |
| (where the owner is a firm or corporation, please print or type the full name of the person signing.) | | | | |
| (| | | | |

NOTE: Unit owners within a Peel Standard Condominium Corporation are to secure authorization from the Directors of the Condominium Corporation in a form satisfactory to the City of Brampton, prior to submission of an application. Signatures from all Members of the Board of Directors are required.

NOTE: If the owner is a firm or corporation, the corporate seal shall be affixed hereto.

PERMISSION TO ENTER

To: The Secretary-Treasurer
Committee of Adjustment
City of Brampton
2 Wellington Street West
Brampton, Ontario
L6Y 4R2
coa@brampton.ca

LOCATION OF THE SUBJECT LAND: 9501-9511 Torbram Road, Brampton

I/We, Williams Parkway Torbram Holdings

please print/type the full name of the owner(s)

the undersigned, being the registered owner(s) of the subject land, hereby authorize the Members of the City of Brampton Committee of Adjustment and City of Brampton staff members, to enter upon the above noted property for the purpose of conducting a site inspection with respect to the attached application for Minor Variance and/or consent.

| Dated this 17 | _ day of | April | , 20 25 | |
|-------------------|---------------|-----------------------------------|--|------------------------------|
| Bul | | | | |
| (signature of the | e owner[s], c | r where the owner is a firm | or corporation, the signature | of an officer of the owner.) |
| | | | | |
| | | | | |
| Brad Keast | | | | |
| / 1 | | floor and a second and the second | and a transfer of the first manager of | the annual almaham V |

(where the owner is a firm or corporation, please print or type the full name of the person signing.)

NOTE: If the owner is a firm or corporation, the corporate seal shall be affixed hereto.

NO DISCUSSION SHALL TAKE PLACE BETWEEN THE COMMITTEE MEMBERS AND THE APPLICANT DURING THE SITE INSPECTION

| | 8. Committee | of Adjustment | |
|-------|---|------------------------------|---|
| 8.1 | Residential* Minor \ *Excluding Apa | Variance Applica | ations |
| 8.1.1 | Above/Below Grade Door/Window related Application | \$11,949 | Per Application |
| 8.1.2 | Driveway and/or Parking related Application | \$11,949 | Per Application |
| 8.1.3 | Variances to Section 10.16 (Additional Residential Units) of the Zoning By-Law | \$11,949 | Per Application |
| 8.1.4 | All Other Variances | \$2,990 | Per Variance to a Maximum Fee of \$11,949 |
| 8.1.5 | "After the Fact" Variance: Variance application resulting from a registered | | |
| 8.1.6 | Maximum Fee | \$11,949 | Per Application |
| 8.2 | Institutional, Commercial, Industrial, Residentia | al Apartment Bui | Iding Minor Variance Applications |
| 8.2.1 | Minor Variance Application | \$11,949 | Per Application |
| 8.3 | Consent A | Applications | |
| 8.3.1 | Consent Application – Lot Creation | \$10,157 | Per Application |
| 8.3.2 | Consent Application – All Other (in accordance with Planning Act S.57, 50(18), or 53(23) | \$5,078 | Per Application |
| 8.3.3 | Consent Certificate | \$2,127 | Per Certificate |
| 8.4 | General Committee of Adjustment Fees | | |
| 8.4.1 | Committee of Adjustment Application Re- Circulated Pursuant to A Request by The Applicant to Defer an Application | 50% of Application Fee | Per Deferral |
| 8.4.2 | Replacement Notice Sign | \$75 | Per Sign |

Committee of Adjustment Application Refunds:

- 75% refund if withdrawn prior to internal circulation 50% refund if withdrawn prior to circulation of public notice of a hearing. No refund if withdrawn once the circulation of the public notice of a hearing has occurred

JUN 11 2025

Committeee of Adjustment



Submitted via email to coa@brampton.ca

April 22, 2025

Updated May 16, 2025

Updated June 10, 2025

Committee of Adjustment City of Brampton 2 Wellington Street West Brampton, ON L6Y 4R2

Attention: Committee of Adjustment

RE: Minor Variance Rationale Letter

9501-9511 Torbram Road TBG Project Number: 24215

INTRODUCTION

On behalf of our client, Williams Parkway Torbram Holdings LP ("the Client"), The Biglieri Group Ltd. ("Planner") is pleased to provide this Minor Variance Rationale Letter for the lands preliminarily addressed as 9501-9511 Torbram Road (severed from 2000 Williams Parkway part of the Stellantis Automotive Assembly Plant) in the City of Brampton ("Subject Site" or "Site"). The proposed Minor Variance is being sought to permit the development of two industrial buildings on a site currently used as auxiliary parking for an existing industrial use.

SUBJECT SITE AND SURROUNDING AREA

The Subject Site (Figure 1) is located at the northeast corner of Williams Parkway and Torbram Road. The Subject Site is 130,297.10 m² in size with an approximate frontage of 685 m on Torbram Road. The parcel was recently created through a severance of surplus land from the adjacent lands at 2000 Williams Parkway. The Subject Site is currently used for paved surface parking. There are sidewalks along North Park Drive, Torbram Road, and Williams Parkway providing pedestrian access to the site. There are bus stops located at Torbram Road and Jardine Street and at Torbram Road and North Park Road serviced by Brampton Transit's Route 14 that connects the site to Malton GO station via Torbram Road. There is also a bus stop located at Williams Parkway at Bramtree Crescent that is serviced by Brampton Transit's Route 29 and provides transit connections to Mount Pleasant GO station and the surrounding neighbourhood via Williams Parkway.

The Subject Site is designated as *Urban System* and *Employment Areas* within the Region of Peel Official Plan and is designated as *Employment* within the Official Plan and General

Employment 1 within the Airport Intermodal Secondary Plan. The Subject Site is zoned *Industrial* (*M2-305*) and *Industrial* (*M4-307*) and Floodplain (*F*) as per the City of Brampton Comprehensive Zoning By-Law 270-2004. It is our recommendation that the *M2-305* zone should apply to the entirely of the site as the *M4-307* was implemented specifically for the preseverance lot for landscaped area. As the site has been created via severance, that zone should be removed and the *M2-305* zone should apply. In addition, a previous Minor Variance application was approved for the site through the Consent application, establishing the following zoning standards that differ from the *M2-305* exception:

- 1. Permit a 9.0 metre street line setback from Torbram Road.
- 2. Permit a landscape buffer having a minimum width of 9.0 metres of continuous, uninterrupted planting strip along Torbram Road.
- 3. Permit a landscape buffer having a minimum width of 60 metres along North Park Drive for a minimum distance of not more or less than 145 metres east of Torbram Road.

The City of Brampton is currently preparing a Comprehensive Zoning By-law. Under the new proposed zoning framework, the subject site is zoned General Employment (GE). At this time, the draft By-law remains under review and is not in-effect.

Surrounding areas:

- North: Employment and industrial uses along North Park Drive, including warehouse uses.
- East: Various Industrial uses, including a vehicle assembly plant.
- > South: Variety of employment and commercial buildings
- > West: Residential subdivision consisting of one and two-storey semi-detached and detached residential dwellings.

DESCRIPTION OF PROPOSAL

The latest development concept for the subject site proposes two one-storey industrial buildings with associated surface parking. Total Gross Floor Area (GFA) for the concept is 63,939 m²; Building 1 has a GFA of 33,237 m² and Building 2 has a GFA of 30,702 m². Both industrial buildings accommodate truck level doors along their eastern faces and a total of 100 trailer parking spaces are provided. Vehicular parking is provided within surface parking areas that are separate from the truck parking area to the north, south, and west of the proposed buildings. A total of 381 vehicular parking spaces are currently included as part of the concept. Access from North Park Drive and Williams Parkway is to be provided via un-signalized limited moves intersections; a signalized, all-moves intersection along Torbram Road in alignment with Jardine Street to the west, is being proposed as part of this concept. This access on Torbram Road would be for typical vehicles only, and not truck trailer movements. The proposed transformers are 23.2 metres and 27.4 metres from the interior side yard. The required setback is 30 metres, and we are proposing a variance of 20 metres to account for any future minor reconfiguration of the transformers during detailed design. Final location is to be confirmed by Alectra Utilities.

POLICY CONTEXT AND PLANNING ANALYSIS

Variance Requested

The Zoning Chart below outlines the proposed variances to the existing zoning by-law.

| Variance Number | Zoning Requirement | Zoning Requirement | Proposed |
|--------------------|-----------------------------|--|--|
| 1 | North Park Drive Setback | 25 metres | 24 metres |
| 2 | Parking Spaces | Building 1 INDUSTRIAL - WAREHOUSE 168 SPACES (FOR FIRST 20,000 m² + 13,109/170 (78 SPACES) = 246 Building 2 INDUSTRIAL - WAREHOUSE 168 SPACES (FOR FIRST 20,000 m² + 8,125/170 (48 SPACES)= 231 Total Required: 477 | Building 1: 174 Building 2: 207 Total: 381 |
| 3 | Height | 10.8 m | 15.3 m (Requested as this is the anticipated height of the proposed development) |
| 4 | Outdoor Storage | (2) outside storage of good, materials and equipment is permitted, provided that such storage areas are located no closer than: (i) 150.0 metres to Williams Parkway; (ii) 90.0 metres to Torbram Road; (iii) 25.0 metres to North Park Drive; and, (iv) 180.0 metres to Airport Road. | (i) 80 m away from Williams (ii) 40 m from Torbram (iii) 100 from North Park Drive |

(3) outside storage areas shall (3) No berm to be provided be provided with a berm not less than 2.4 metres in height which is effective in screening the storage area from North Park Drive, Williams Parkway and from Airport Road, except in the case where a grade separation facility requires that Airport Road shall be elevated, and in the case of North Park Drive, outside storage shall be screened by a fence 2.4 metres in height Landscape (1) a minimum width of 30.0 (1) Landscaping minimum of 5 Buffers Williams 3.5 metres along metres along Parkway; Parkway To permit a landscape buffer (3) A landscape strip of 9 m having a minimum width of 60 along North Park Drive with no metres along North Park Drive minimum distance. for a minimum distance of not more or less than 145 metres east of Torbram Road 6 Transformer Internal side yard setback Proposed variance of 20 metre 30 metres Setbacks minimum yard setback.

Planning Act

Pursuant to Section 45(1) of the Planning Act, R.S.O. 1990, c. P.13 ("the Act"), The Committee of Adjustment can approve minor variances to Zoning By-laws for land, buildings, or structures upon the Owner's request, if they believe the changes are in keeping with the general purpose and intent of the Official Plan and Zoning By-law; are desirable for the appropriate development or use of the land, building or structure; and are minor in nature (the "Four Tests"). The variances must also be consistent with the Provincial Planning Statement, 2024 ("PPS").

Provincial Planning Statement (2024)

In accordance with Section 5 of the Planning Act, all decisions must be consistent with policies found within policy statements passed under Section 3 of the Act. The Provincial Planning Statement ("PPS") (2024) is a policy document passed under Section 3, and therefore decisions must be consistent with the policies therein. The PPS provides provincial direction on matters related to land use planning including growth, housing supply, economic development, and the protection of natural and cultural heritage resources.

FOUR TESTS ANALYSIS

It is TBG's professional planning opinion that the proposed variances satisfy the four tests stipulated by The Planning Act as follows:

The requested variances meet the four tests stipulated by The Planning Act as outlined below:

1) Is the general intent and purpose of the Official Plan maintained?

The Region of Peel Official Plan ("RPOP") provides policy direction for sustainable development that weaves together environment, social, economic, and cultural priorities. The Subject Site is designated as *Urban System* and *Employment Areas* within the RPOP. Section 5.8 provides policy direction for *Employment Areas*. *Employment Areas* are identified in the RPOP as lands that are to be protected for a range of employment uses and encourage more intensive use of land close to current and planned transit routes. Policy direction within the RPOP is supportive of intensification of existing Employment Areas with compatible employment uses (Policy 5.8.23).

The City of Brampton Official Plan ("OP") provides policy direction for the strategic planning for the rapid growth anticipated in the City. The Subject Site is designated as *Employment* within the OP. The OP encourages *Employment Areas* to be preserved for growth and intensification (Policy 2.2.8.1.a). Policy stipulates that employment areas should be planned and designed to be easily accessible via a range of transportation modes (Policy 2.1.2.36). Policy also stipulates that a diverse range of job opportunities should be accommodated within proximity to major goods movement infrastructure including Pearson Airport (Policy 2.1.2.33). Torbram Road and Williams Parkway are also identified as *Support Corridors* as per schedule 1A of the OP. Policy encourages infill opportunities within this area and buildings that are taller in height, up to 4 storeys (low rise plus) within 800 m of the *Support Corridor* (Policy 2.2.5.14).

The Airport Intermodal Secondary Plan Area ("AISPA") provides a land use policy framework for the employment area north of Lester B. Pearson International Airport. The AISPA designates the

site as *General Employment 1*. The AISPA states that the General Employment 1 designation may permit a broad range of industrial uses including warehousing, and storage of goods, manufacturing, processing, repairing and servicing operations, and outdoor storage area as an accessory to an industrial use.

Variance 1

Variance 1 aligns with the RPOP, OP, and AISPA as the proposed development supports infill of *Employment Lands* and will contribute to Brampton's future growth and intensification targets for employment.

The proposed setback variance for North Park Drive is required as the in-place zoning for the site was based on the original parcel configuration. Under the current zoning, the required street line setback from Williams Parkway is 30 metres. From that street line, the proposed setback is 57.9 metres. At the closest location of the property line, which is not the street line, the proposed setback is 20.8 metres. Based on the language of the zoning in-place, it is our opinion that a variance for the setback from Williams Parkway is not required. The in-place zoning standards for the Torbram Road and interior side yard setbacks are complied with.

Now that the new development parcel has been created through the consent process, relief from the applicable site specific zoning is required to implement zoning provisions that will facilitate development on the new parcel. As such, it is our opinion that the setback variances requested for North Park Drive conform with the RPOP and OP and that the general intent and purpose is maintained.

Variance 2

The reduction of parking spaces allows for an increased footprint to accommodate growth targets for *Employment Lands*. Additionally, both the OP and the RPOP provide policy direction for the development of Employment uses within proximity to transit. The reduction of parking spaces promotes transit use and encourages carpooling amongst employees. In addition, the reduction in parking area allows for the provision of generous landscape strips around the periphery of the site. As such, it is our opinion that the variance for parking reduction conforms with the RPOP and OP and that the general intent and purpose is maintained.

In addition, a Parking Study has been prepared by TYLin and is enclosed, which provides justification for the reduced parking rate. Proxy sites included 155-161 Orenda Road and 250 Chrysler Drive. TYLin concludes that based on the observed utilization of the proxy sites, and the ITE Parking Generation Manual, a variance to permit the proposed parking supply of 381 parking spaces is appropriate.

Variance 3

While a variance is required for the proposed height of 15.3 metres, the overall height and massing of the site is consistent with the OP's policy direction for height within *Support Corridors*. It is our opinion that the proposed variance for height conforms to the RPOP and OP and that the general intent and purpose is maintained.

Variance 4

The setbacks for outdoor storage are in keeping with the general intent and purpose of Employment land use in the RPOP and OP. The setbacks provide ample distance between the

public realm and the parking areas. Appropriate screening has been incorporated into the site design to provide visual mitigation. A 2.4 metre high fence is provided at the north end of the site, in proximity of North Park Drive, and a second fence also with a height of 2.4 metres is provided at the south end of the site in proximity of Williams Parkway. A third wall with a height of 3.4 metres is provided in proximity of the proposed entrance along Torbram Road, which also provides noise mitigation. As such, the proposed outdoor storage area is appropriately located and screened from view.

In addition, the previous berm that was provided for the site prior to the severance will not be maintained. It is our opinion that a berm is not required to provide suitable screening. The setback distance, landscaped buffers, and planting will provide the appropriate visual mitigation to screen the storage areas from view.

It is our opinion that the proposed variance for outdoor storage conforms to the RPOP and OP and that the general intent and purpose is maintained.

Variance 5

The proposed landscaping buffers are in keeping with the general intent of the RPOP and OP as they provide separation from the Right-of-Ways and assist in providing visual mitigation. Through the previous minor variance application for the lands to facilitate the severance, a minimum landscape strip along Torbram Road of 9 metres was approved. The proposed site plan incorporates similar landscaped strips along the Williams and North Park Drive frontages. A Landscape Plan has been prepared by Studio TLA, which provides detail on the 199 trees that are to be planted around the perimeter of the site within the proposed landscape strips. These trees will serve as visual mitigation and will aid in replacing many of the trees that need to be removed from the site to permit the proposed development. It is our opinion that the proposed variance related to the reduction of landscape strips along North Park Drive and Williams Parkway conforms to the Official Plans.

Variance 6

The proposed transformers are 23.2 metres and 27.4 metres from the interior side yard. The required setback is 30 metres, and we are proposing a variance of 20 metres to account for any future minor reconfiguration of the transformers during detailed design. The proposed minimum setback for transformers maintains the intent and purpose of the Official Plan. The transformers will also confirm with all building code related requirements.

2) Is the general intent and purpose of the Zoning By-law maintained?

The Subject Site is zoned *Industrial (M2-305)* and *Industrial (M4-307)* and Floodplain (F) as per the City of Brampton Comprehensive Zoning By-Law 270-2004. The applicable zoning facilitated development of the full pre-severed lot, namely the Chrysler plant. As mentioned above, it is our recommendation that the M2-305 zone should apply to the entirely of the site as the M4-307 was implemented specifically for the pre-severance lot for landscaped area. As the site has been created via severance, that zone should be removed and the M2-305 zone should apply. Alternatively, all of the provisions to be implemented through this proposed variance and the applicable zoning within M2-305 zone should be transferred to the M4-307 exception.

Variance 1

The current zoning requires a 25-meter setback from North Park Drive. The proposed development contemplates a 24.3-metre setback, and as such a 24 metre variance is being requested. The Zoning By-Law 270-2004 requires buildings in the M2 parent zone to have a minimum rear lot line of 7 metres. Given that the site is a through lot and corner lot per the City's zoning definition, the shorter of the lot lines is deemed the front lot line, and as such the North Park Drive setback is deemed to be the rear year. Therefore, the proposed variance to permit a setback of 24 metres exceeds what is typically required for industrial sites zoned M2 within the City of Brampton. The proposed setback is aligned with the general intent of the Zoning By-law for Industrial uses.

Variance 2

The current zoning requires 168 parking spaces for buildings over 20,000 square metres plus an additional parking space per 170 square metres of gross floor area (GFA). This calculation would require 246 parking spaces for Building A and 231 parking spaces for Building B. The proposed development would provide 174 parking spaces for Building A and 207 parking spaces for Building B, for a supply of 381. As noted above, a Parking Study has been prepared which demonstrates that the proposal will provide an appropriate supply of parking to meet demand. As such, the reduced parking is aligned with the general intent of the Zoning By-law and provides an appropriate amount of parking for the subject use of the Site.

Variance 3

The current zoning stipulates that the maximum building height is 10.8 metres. The proposed development contemplates a height of 15.3 metres (updated elevations to be provided). Zoning By-Law 270-2004 does not prescribe a limit in metres, however, heights are limited to two storeys on sites that abut a Residential zone. Both proposed buildings will be one storey in height. As a result, the height of the proposed development is aligned with the general intent of the Zoning By-law.

Variance 4

The current zoning stipulates that outdoor storage must be setback 150 metres to Williams Parkway, 90 metres to Torbram Road, and 25 metres to North Park Drive. The proposed development contemplates an outdoor storage setback of 80 metres from Williams Parkway and 40 metres from Torbram Road. As described above, the proposal incorporates multiple visual barriers to screen the outdoor storage area from the view of the public realm. In addition, the *M2* parent zone does not establish minimum distances of outdoor storage areas from property lines and only required that storage be screened and located within rear or interior side yards. In addition, there is no requirement of a berm to provide for visual mitigation within the parent zoning. As such, the proposed distances between outdoor storage and property lines, and the proposed removal of the existing berm meets the general intent and purpose of the Zoning By-law.

Variance 5

The current zoning stipulates that there should be a minimum landscape buffer of 30.0 metres along Williams Parkway, a 9 metre strip along Torbram Road, and a 60 metre strip along North Park Drive with a distance of 145 metres. The proposed development proposes a 3.5 metre strip of landscaping along Williams Parkway, a 9 metre landscape strip along North Park Drive, and a 9 metre landscape strip along Torbram, which is in compliance with the previous Minor Variance approval for the site. Required minimum landscaping strips under the *M2* parent zone are 3

metres abutting a street or industrial use, and 6 metres abutting any Residential zone. As such, the proposed landscape strips meet the general intent and purpose of the Zoning By-law as the proposed landscape strips exceed what is required within the parent M2 zone. In addition the reduced landscape strips are suitable as they are still sized in order to facilitate substantial deciduous and coniferous tree plantings to provide ample visual mitigation.

Variance 6

The proposed transformers are 23.2 metres and 27.4 metres from the interior side yard. The required setback is 30 metres, and we are proposing a variance of 20 metres to account for any future minor reconfiguration of the transformers during detailed design and based on Alectra's review. The transformers will also conform with all building code-related requirements. The transformers are located a sufficient distance from the public realm and will be mostly screened from view by proposed plantings. See enclosed landscape plan for detail. As such, the proposed minimum setback for both transformers maintains the intent and purpose of the Zoning By-law.

3) Are the proposed variances appropriate for the development of the land and/or building?

The proposed variances are appropriate for the development of the land. The zoning for this land was initially established for the land's use as an automotive assembly plant. In 2024, excess lands belonging to the owner of the assembly plant were severed through a consent, and are now proposed to be used for two new industrial buildings as described above. The reduced setbacks for the building and outdoor storage, reduction of parking, increased height, and reduction in landscape buffers are all appropriate for the development of the land for industrial use and are appropriate for the proposed building type. In addition, opo. The transformer will also be designed in conformance with the Building Code and in consultation with Alectra. The zoning standards contemplated through this application are consistent with the zoning standards that are implemented for other warehouse type uses throughout the City of Brampton. In addition, several of the proposed standards exceed the minimum requirements within the *M2* parent zone, as noted above.

For the reasons outlined in this letter, it is our professional planning opinion that the proposed Minor Variance is appropriate for the development of the land and building.

4) Are the proposed variances minor?

While the magnitude of the zoning relief required may not appear 'minor' it is important to review the proposed variances in the context of the current zoning which was implemented for presevered lot. Looking at the nature of the variances in the context of the newly created development site, the variances are minor in nature as they conform to the general intent of the Official plan, Zoning By-law, facilitate suitable development of two new industrial buildings within an industrial land use and do not negatively impact the public. The reduced setbacks (Variances 1) will permit the infilling of employment uses, ensuring more efficient land use and economic activity within the City.

Variance 2 will reduce the number of parking spaces; this reduction is minor and will be offset by proximity to transit routes. In addition, a detailed Parking Study has been prepared by TyLin supporting the reduction through review of proxy sites.

Variance 3 will increase the maximum height by 3.51 metres. In our opinion, the height variance is justified by the generous building setbacks and landscaping that will provide screening and minimize the visual impact of the buildings.

Variance 4 contemplates the decreased storage setbacks, appropriate screening and setbacks are being provided to mitigate the impact of the outdoor storage.

Variance 5 contemplates reduced landscape buffers, while the landscape buffers would be reduced through the proposed development, the landscape plan includes trees and shrubbery that will mitigate the visual impact of the site minimizing the effect of the reduced landscaped buffers.

Finally, Variance 6 for the proposed setback distance of 20 metres for the interior side yard for both transformers is minor and will not negatively impact neighbouring landowners or the public.

Faith Eagling of Albert

The transformers will be screened by vegetation and will be designed in compliance with the Building Code and Alectra.

The site has been designed to have negligible impacts on the immediate neighbouring lands and the public realm. Care has been taken to ensure that the proposed development will ensure a suitable massing and scale of development, as well as a comfortable public realm.

For the reasons outlined in this letter, it is our professional opinion that the proposed Minor Variances are minor in nature.

In addition, the proposed transformer locations will not require variances should the proposed setback for North Park Drive be approved. Neither transformer locations are located within any required yard setback, in compliance with section 6.10 of Zoning By-law 270 – 2004.

CLOSING

It is our professional Planning Opinion that the application meets the four tests of minor variance and should be approved by the Committee of Adjustment.

We trust you will find all in order, however if you have any questions or require additional information, please contact us at your earliest convenience.

Respectfully,

THE BIGLIERI GROUP LTD.

Mallory Nievas, MES, MCIP, RPP

Associate

Leyla Mains, MA

Planne



Submitted via email to coa@brampton.ca

April 22, 2025

Updated May 16, 2025

Committee of Adjustment City of Brampton 2 Wellington Street West Brampton, ON L6Y 4R2

Attention: Committee of Adjustment

RE: Minor Variance Rationale Letter

9501-9511 Torbram Road TBG Project Number: 24215

INTRODUCTION

On behalf of our client, Williams Parkway Torbram Holdings LP ("the Client"), The Biglieri Group Ltd. ("Planner") is pleased to provide this Minor Variance Rationale Letter for the lands preliminarily addressed as 9501-9511 Torbram Road (severed from 2000 Williams Parkway part of the Stellantis Automotive Assembly Plant) in the City of Brampton ("Subject Site" or "Site"). The proposed Minor Variance is being sought to permit the development of two industrial buildings on a site currently used as auxiliary parking for an existing industrial use.

SUBJECT SITE AND SURROUNDING AREA

The Subject Site (**Figure 1**) is located at the northeast corner of Williams Parkway and Torbram Road. The Subject Site is 130,297.10 m² in size with an approximate frontage of 685 m on Torbram Road. The parcel was recently created through a severance of surplus land from the adjacent lands at 2000 Williams Parkway. The Subject Site is currently used for paved surface parking. There are sidewalks along North Park Drive, Torbram Road, and Williams Parkway providing pedestrian access to the site. There are bus stops located at Torbram Road and Jardine Street and at Torbram Road and North Park Road serviced by Brampton Transit's Route 14 that connects the site to Malton GO station via Torbram Road. There is also a bus stop located at Williams Parkway at Bramtree Crescent that is serviced by Brampton Transit's Route 29 and provides transit connections to Mount Pleasant GO station and the surrounding neighbourhood via Williams Parkway.

The Subject Site is designated as *Urban System* and *Employment Areas* within the Region of Peel Official Plan and is designated as *Employment* within the Official Plan and General Employment 1 within the Airport Intermodal Secondary Plan. The Subject Site is zoned *Industrial (M2-305)* and *Industrial (M4-307)* and Floodplain *(F)* as per the City of Brampton

Comprehensive Zoning By-Law 270-2004. It is our recommendation that the M2-305 zone should apply to the entirely of the site as the M4-307 was implemented specifically for the preseverance lot for landscaped area. As the site has been created via severance, that zone should be removed and the M2-305 zone should apply. In addition, a previous Minor Variance application was approved for the site through the Consent application, establishing the following zoning standards that differ from the M2-305 exception:

- 1. Permit a 9.0 metre street line setback from Torbram Road.
- 2. Permit a landscape buffer having a minimum width of 9.0 metres of continuous, uninterrupted planting strip along Torbram Road.
- 3. Permit a landscape buffer having a minimum width of 60 metres along North Park Drive for a minimum distance of not more or less than 145 metres east of Torbram Road.

The City of Brampton is currently preparing a Comprehensive Zoning By-law. Under the new proposed zoning framework, the subject site is zoned General Employment (GE). At this time, the draft By-law remains under review and is not in-effect.

Surrounding areas:

- North: Employment and industrial uses along North Park Drive, including warehouse uses.
- **East:** Various Industrial uses, including a vehicle assembly plant.
- > South: Variety of employment and commercial buildings
- > West: Residential subdivision consisting of one and two-storey semi-detached and detached residential dwellings.

DESCRIPTION OF PROPOSAL

The latest development concept for the subject site proposes two one-storey industrial buildings with associated surface parking. Total Gross Floor Area (GFA) for the concept is 63,939 m²; Building 1 has a GFA of 33,237 m² and Building 2 has a GFA of 30,702 m². Both industrial buildings accommodate truck level doors along their eastern faces and a total of 100 trailer parking spaces are provided. Vehicular parking is provided within surface parking areas that are separate from the truck parking area to the north, south, and west of the proposed buildings. A total of 381 vehicular parking spaces are currently included as part of the concept. Access from North Park Drive and Williams Parkway is to be provided via un-signalized limited moves intersections; a signalized, all-moves intersection along Torbram Road in alignment with Jardine Street to the west, is being proposed as part of this concept. This access on Torbram Road would be for typical vehicles only, and not truck trailer movements. The proposed transformers are 23.2 metres and 27.4 metres from the interior side yard. The required setback is 30 metres, and we are proposing a variance of 20 metres to account for any future minor reconfiguration of the transformers during detailed design. Final location is to be confirmed by Alectra Utilities.

POLICY CONTEXT AND PLANNING ANALYSIS

Variance Requested

The Zoning Chart below outlines the proposed variances to the existing zoning by-law.

| Variance Number | Zoning Requirement | Zoning Requirement | Proposed |
|--------------------|-----------------------------|--|--|
| 1 | North Park Drive Setback | 25 metres | 24 metres |
| 2 | Parking Spaces | Building 1 INDUSTRIAL - WAREHOUSE 168 SPACES (FOR FIRST 20,000 m² + 13,109/170 (78 SPACES) = 246 Building 2 INDUSTRIAL - WAREHOUSE 168 SPACES (FOR FIRST 20,000 m² + 8,125/170 (48 SPACES)= 231 | Building 1: 174 Building 2: 207 Total: 381 |
| 3 | Height | Total Required: 477 | 14.31 m |
| 4 | Outdoor Storage | (2) outside storage of good, materials and equipment is permitted, provided that such storage areas are located no closer than: (i) 150.0 metres to Williams Parkway; (ii) 90.0 metres to Torbram Road; (iii) 25.0 metres to North Park Drive; and, (iv) 180.0 metres to Airport Road. | (2) Outdoor storage is approx. (i)80 m away from Williams (ii) 40 m from Torbram |

- (3) outside storage areas shall (3) No berm to be provided be provided with a berm not less than 2.4 metres in height which is effective in screening the storage area from North Park Drive, Williams Parkway and from Airport Road, except in the case where a grade separation facility requires that Airport Road shall be elevated, and in the case of North Park Drive, outside storage shall be screened by a fence 2.4 metres in height

- 5 Landscape Buffers
- (1) a minimum width of 30.0 (1) Landscaping minimum of metres along Parkway;
 - Williams 3.5 metres along Williams Parkway

metres along North Park Drive minimum distance. for a minimum distance of not more or less than 145 metres east of Torbram Road

To permit a landscape buffer (3) A landscape strip of 9 m having a minimum width of 60 along North Park Drive with no

| 6 | Transformer Setbacks | Internal side yard setback 30 metres | Proposed variance of 20 metre minimum yard setback. |
|---|-------------------------|---|---|
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Planning Act

Pursuant to Section 45(1) of the Planning Act, R.S.O. 1990, c. P.13 ("the Act"), The Committee of Adjustment can approve minor variances to Zoning By-laws for land, buildings, or structures upon the Owner's request, if they believe the changes are in keeping with the general purpose and intent of the Official Plan and Zoning By-law; are desirable for the appropriate development or use of the land, building or structure; and are minor in nature (the "Four Tests"). The variances must also be consistent with the Provincial Planning Statement, 2024 ("PPS").

Provincial Planning Statement (2024)

In accordance with Section 5 of the Planning Act, all decisions must be consistent with policies found within policy statements passed under Section 3 of the Act. The Provincial Planning Statement ("PPS") (2024) is a policy document passed under Section 3, and therefore decisions must be consistent with the policies therein. The PPS provides provincial direction on matters related to land use planning including growth, housing supply, economic development, and the protection of natural and cultural heritage resources.

FOUR TESTS ANALYSIS

It is TBG's professional planning opinion that the proposed variances satisfy the four tests stipulated by The Planning Act as follows:

The requested variances meet the four tests stipulated by The Planning Act as outlined below:

1) Is the general intent and purpose of the Official Plan maintained?

The Region of Peel Official Plan ("RPOP") provides policy direction for sustainable development that weaves together environment, social, economic, and cultural priorities. The Subject Site is designated as *Urban System* and *Employment Areas* within the RPOP. Section 5.8 provides policy direction for *Employment Areas*. *Employment Areas* are identified in the RPOP as lands that are to be protected for a range of employment uses and encourage more intensive use of land close to current and planned transit routes. Policy direction within the RPOP is supportive of intensification of existing Employment Areas with compatible employment uses (Policy 5.8.23).

The City of Brampton Official Plan ("OP") provides policy direction for the strategic planning for the rapid growth anticipated in the City. The Subject Site is designated as *Employment* within the OP. The OP encourages *Employment Areas* to be preserved for growth and intensification (Policy 2.2.8.1.a). Policy stipulates that employment areas should be planned and designed to be easily accessible via a range of transportation modes (Policy 2.1.2.36). Policy also stipulates that a diverse range of job opportunities should be accommodated within proximity to major goods movement infrastructure including Pearson Airport (Policy 2.1.2.33). Torbram Road and Williams Parkway are also identified as *Support Corridors* as per schedule 1A of the OP. Policy encourages infill opportunities within this area and buildings that are taller in height, up to 4 storeys (low rise plus) within 800 m of the *Support Corridor* (Policy 2.2.5.14).

The Airport Intermodal Secondary Plan Area ("AISPA") provides a land use policy framework for the employment area north of Lester B. Pearson International Airport. The AISPA designates the site as *General Employment 1*. The AISPA states that the General Employment 1 designation may permit a broad range of industrial uses including warehousing, and storage of goods, manufacturing, processing, repairing and servicing operations, and outdoor storage area as an accessory to an industrial use.

Variance 1

Variance 1 aligns with the RPOP, OP, and AISPA as the proposed development supports infill of Employment Lands and will contribute to Brampton's future growth and intensification targets for employment.

The proposed setback variance for North Park Drive is required as the in-place zoning for the site was based on the original parcel configuration. Under the current zoning, the required street line setback from Williams Parkway is 30 metres. From that street line, the proposed setback is 57.9 metres. At the closest location of the property line, which is not the street line, the proposed setback is 20.8 metres. Based on the language of the zoning in-place, it is our opinion that a variance for the setback from Williams Parkway is not required. The in-place zoning standards for the Torbram Road and interior side yard setbacks are complied with.

Now that the new development parcel has been created through the consent process, relief from the applicable site specific zoning is required to implement zoning provisions that will facilitate development on the new parcel. As such, it is our opinion that the setback variances requested for North Park Drive conform with the RPOP and OP and that the general intent and purpose is maintained.

Variance 2

The reduction of parking spaces allows for an increased footprint to accommodate growth targets for *Employment Lands*. Additionally, both the OP and the RPOP provide policy direction for the development of Employment uses within proximity to transit. The reduction of parking spaces promotes transit use and encourages carpooling amongst employees. In addition, the reduction in parking area allows for the provision of generous landscape strips around the periphery of the site. As such, it is our opinion that the variance for parking reduction conforms with the RPOP and OP and that the general intent and purpose is maintained.

In addition, a Parking Study has been prepared by TYLin and is enclosed, which provides justification for the reduced parking rate. Proxy sites included 155-161 Orenda Road and 250

Chrysler Drive. TYLin concludes that based on the observed utilization of the proxy sites, and the ITE Parking Generation Manual, a variance to permit the proposed parking supply of 381 parking spaces is appropriate.

Variance 3

While a variance is required for the proposed height of 14.31, the overall height and massing of the site is consistent with the OP's policy direction for height within *Support Corridors*. It is our opinion that the proposed variance for height conforms to the RPOP and OP and that the general intent and purpose is maintained.

Variance 4

The setbacks for outdoor storage are in keeping with the general intent and purpose of Employment land use in the RPOP and OP. The setbacks provide ample distance between the public realm and the parking areas. Appropriate screening has been incorporated into the site design to provide visual mitigation. A 2.4 metre high fence is provided at the north end of the site, in proximity of North Park Drive, and a second fence also with a height of 2.4 metres is provided at the south end of the site in proximity of Williams Parkway. A third wall with a height of 3.4 metres is provided in proximity of the proposed entrance along Torbram Road, which also provides noise mitigation. As such, the proposed outdoor storage area is appropriately located and screened from view.

In addition, the previous berm that was provided for the site prior to the severance will not be maintained. It is our opinion that a berm is not required to provide suitable screening. The setback distance, landscaped buffers, and planting will provide the appropriate visual mitigation to screen the storage areas from view.

It is our opinion that the proposed variance for outdoor storage conforms to the RPOP and OP and that the general intent and purpose is maintained.

Variance 5

The proposed landscaping buffers are in keeping with the general intent of the RPOP and OP as they provide separation from the Right-of-Ways and assist in providing visual mitigation. Through the previous minor variance application for the lands to facilitate the severance, a minimum landscape strip along Torbram Road of 9 metres was approved. The proposed site plan incorporates similar landscaped strips along the Williams and North Park Drive frontages. A Landscape Plan has been prepared by Studio TLA, which provides detail on the 199 trees that are to be planted around the perimeter of the site within the proposed landscape strips. These trees will serve as visual mitigation and will aid in replacing many of the trees that need to be removed from the site to permit the proposed development. It is our opinion that the proposed variance related to the reduction of landscape strips along North Park Drive and Williams Parkway conforms to the Official Plans.

Variance 6

The proposed transformers are 23.2 metres and 27.4 metres from the interior side yard. The required setback is 30 metres, and we are proposing a variance of 20 metres to account for any future minor reconfiguration of the transformers during detailed design. The proposed minimum

setback for transformers maintains the intent and purpose of the Official Plan. The transformers will also confirm with all building code related requirements.

2) Is the general intent and purpose of the Zoning By-law maintained?

The Subject Site is zoned *Industrial (M2-305)* and *Industrial (M4-307)* and Floodplain (F) as per the City of Brampton Comprehensive Zoning By-Law 270-2004. The applicable zoning facilitated development of the full pre-severed lot, namely the Chrysler plant. As mentioned above, it is our recommendation that the M2-305 zone should apply to the entirely of the site as the M4-307 was implemented specifically for the pre-severance lot for landscaped area. As the site has been created via severance, that zone should be removed and the M2-305 zone should apply. Alternatively, all of the provisions to be implemented through this proposed variance and the applicable zoning within M2-305 zone should be transferred to the M4-307 exception.

Variance 1

The current zoning requires a 25-meter setback from North Park Drive. The proposed development contemplates a 24.3-metre setback, and as such a 24 metre variance is being requested. The Zoning By-Law 270-2004 requires buildings in the *M2* parent zone to have a minimum rear lot line of 7 metres. Given that the site is a through lot and corner lot per the City's zoning definition, the shorter of the lot lines is deemed the front lot line, and as such the North Park Drive setback is deemed to be the rear year. Therefore, the proposed variance to permit a setback of 24 metres exceeds what is typically required for industrial sites zoned *M2* within the City of Brampton. The proposed setback is aligned with the general intent of the Zoning By-law for Industrial uses.

Variance 2

The current zoning requires 168 parking spaces for buildings over 20,000 square metres plus an additional parking space per 170 square metres of gross floor area (GFA). This calculation would require 246 parking spaces for Building A and 231 parking spaces for Building B. The proposed development would provide 174 parking spaces for Building A and 207 parking spaces for Building B, for a supply of 381. As noted above, a Parking Study has been prepared which demonstrates that the proposal will provide an appropriate supply of parking to meet demand. As such, the reduced parking is aligned with the general intent of the Zoning By-law and provides an appropriate amount of parking for the subject use of the Site.

Variance 3

The current zoning stipulates that the maximum building height is 10.8 metres. The proposed development contemplates a height of 14.31 metres. Zoning By-Law 270-2004 does not prescribe a limit in metres, however, heights are limited to two storeys on sites that abut a Residential zone. Both proposed buildings will be one storey in height. As a result, the height of the proposed development is aligned with the general intent of the Zoning By-law.

Variance 4

The current zoning stipulates that outdoor storage must be setback 150 metres to Williams Parkway, 90 metres to Torbram Road, and 25 metres to North Park Drive. The proposed development contemplates an outodoor storage setback of 80 m from Williams Parkway and 40 metres from Torbram Road. As described above, the proposal incorporates multiple visual barriers to screen the outdoor storage area from the view of the public realm. In addition, the *M2*

parent zone does not establish minimum distances of outdoor storage areas from property lines and only required that storage be screened and located within rear or interior side yards. In addition, there is no requirement of a berm to provide for visual mitigation within the parent zoning. As such, the proposed distances between outdoor storage and property lines, and the proposed removal of the existing berm meets the general intent and purpose of the Zoning By-law.

Variance 5

The current zoning stipulates that there should be a minimum landscape buffer of 30.0 metres along Williams Parkway, a 9 metre strip along Torbram Road, and a 60 metre strip along North Park Drive with a distance of 145 metres. The proposed development proposes a 3.5 metre strip of landscaping along Williams Parkway, a 9 metre landscape strip along North Park Drive, and a 9 metre landscape strip along Torbram, which is in compliance with the previous Minor Variance approval for the site. Required minimum landscaping strips under the *M2* parent zone are 3 metres abutting a street or industrial use, and 6 metres abutting any Residential zone. As such, the proposed landscape strips meet the general intent and purpose of the Zoning By-law as the proposed landscape strips exceed what is required within the parent *M2* zone. In addition the reduced landscape strips are suitable as they are still sized in order to facilitate substantial deciduous and coniferous tree plantings to provide ample visual mitigation.

Variance 6

The proposed transformers are 23.2 metres and 27.4 metres from the interior side yard. The required setback is 30 metres, and we are proposing a variance of 20 metres to account for any future minor reconfiguration of the transformers during detailed design and based on Alectra's review. The transformers will also conform with all building code-related requirements. The transformers are located a sufficient distance from the public realm and will be mostly screened from view by proposed plantings. See enclosed landscape plan for detail. As such, the proposed minimum setback for both transformers maintains the intent and purpose of the Zoning By-law.

3) Are the proposed variances appropriate for the development of the land and/or building?

The proposed variances are appropriate for the development of the land. The zoning for this land was initially established for the land's use as an automotive assembly plant. In 2024, excess lands belonging to the owner of the assembly plant were severed through a consent, and are now proposed to be used for two new industrial buildings as described above. The reduced setbacks for the building and outdoor storage, reduction of parking, increased height, and reduction in landscape buffers are all appropriate for the development of the land for industrial use and are appropriate for the proposed building type. In addition, the proposed transformer setbacks from the interior lot line are appropriate given the distance from the public realm and the proposed plantings that will minimize visual impact. The transportation will also be designed in conformance with the Building Code and in consultation with Alectra. The zoning standards contemplated through this application are consistent with the zoning standards that are implemented for other warehouse type uses throughout the City of Brampton. In addition, several of the proposed standards exceed the minimum requirements within the *M2* parent zone, as noted above.

For the reasons outlined in this letter, it is our professional planning opinion that the proposed Minor Variance is appropriate for the development of the land and building.

4) Are the proposed variances minor?

While the magnitude of the zoning relief required may not appear 'minor' it is important to review the proposed variances in the context of the current zoning which was implemented for presevered lot. Looking at the nature of the variances in the context of the newly created development site, the variances are minor in nature as they conform to the general intent of the Official plan, Zoning By-law, facilitate suitable development of two new industrial buildings within an industrial land use and do not negatively impact the public. The reduced setbacks (Variances 1) will permit the infilling of employment uses, ensuring more efficient land use and economic activity within the City.

Variance 2 will reduce the number of parking spaces; this reduction is minor and will be offset by proximity to transit routes. In addition, a detailed Parking Study has been prepared by TyLin supporting the reduction through review of proxy sites.

Variance 3 will increase the maximum height by 3.51 metres. In our opinion, the height variance is justified by the generous building setbacks and landscaping that will provide screening and minimize the visual impact of the buildings.

Variance 4 contemplates the decreased storage setbacks, appropriate screening and setbacks are being provided to mitigate the impact of the outdoor storage.

Variance 5 contemplates reduced landscape buffers, while the landscape buffers would be reduced through the proposed development, the landscape plan includes trees and shrubbery that will mitigate the visual impact of the site minimizing the effect of the reduced landscaped buffers.

Finally, Variance 6 for the proposed setback distance of 20 metres for the interior side yard for both transformers is minor and will not negatively impact neighbouring landowners or the public.

The transformers will be screened by vegetation and will be designed in compliance with the Building Code and Alectra.

The site has been designed to have negligible impacts on the immediate neighbouring lands and the public realm. Care has been taken to ensure that the proposed development will ensure a suitable massing and scale of development, as well as a comfortable public realm.

For the reasons outlined in this letter, it is our professional opinion that the proposed Minor Variances are minor in nature.

In addition, the proposed transformer locations will not require variances should the proposed setback for North Park Drive be approved. Neither transformer locations are located within any required yard setback, in compliance with section 6.10 of Zoning By-law 270 – 2004.

CLOSING

It is our professional Planning Opinion that the application meets the four tests of minor variance and should be approved by the Committee of Adjustment.

We trust you will find all in order, however if you have any questions or require additional information, please contact us at your earliest convenience.

Respectfully,

THE BIGLIERI GROUP LTD.

Mallory Nievas, MES, MCIP, RPP

Associate

Leyľa Mains, MA

Planner



Submitted via email to coa@brampton.ca

April 22, 2025

Committee of Adjustment City of Brampton 2 Wellington Street West Brampton, ON L6Y 4R2

Attention: Committee of Adjustment

RE: Minor Variance Rationale Letter

9501-9511 Torbram Road TBG Project Number: 24215

INTRODUCTION

On behalf of our client, Williams Parkway Torbram Holdings LP ("the Client"), The Biglieri Group Ltd. ("Planner") is pleased to provide this Minor Variance Rationale Letter for the lands preliminarily addressed as 9501-9511 Torbram Road (severed from 2000 Williams Parkway part of the Stellantis Automotive Assembly Plant) in the City of Brampton ("Subject Site" or "Site"). The proposed Minor Variance is being sought to permit the development of two industrial buildings on a site currently used as auxiliary parking for an existing industrial use.

SUBJECT SITE AND SURROUNDING AREA

The Subject Site (**Figure 1**) is located at the northeast corner of Williams Parkway and Torbram Road. The Subject Site is 130,297.10 m² in size with an approximate frontage of 685 m on Torbram Road. The parcel was recently created through a severance of surplus land from the adjacent lands at 2000 Williams Parkway. The Subject Site is currently used for paved surface parking. There are sidewalks along North Park Drive, Torbram Road, and Williams Parkway providing pedestrian access to the site. There are bus stops located at Torbram Road and Jardine Street and at Torbram Road and North Park Road serviced by Brampton Transit's Route 14 that connects the site to Malton GO station via Torbram Road. There is also a bus stop located at Williams Parkway at Bramtree Crescent that is serviced by Brampton Transit's Route 29 and provides transit connections to Mount Pleasant GO station and the surrounding neighbourhood via Williams Parkway.

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severance lot for landscaped area. As the site has been created via severance, that zone should be removed and the M2-305 zone should apply. In addition, a previous Minor Variance application was approved for the site through the Consent application, establishing the following zoning standards that differ from the M2-305 exception:

- 1. Permit a 9.0 metre street line setback from Torbram Road.
- 2. Permit a landscape buffer having a minimum width of 9.0 metres of continuous, uninterrupted planting strip along Torbram Road.
- 3. Permit a landscape buffer having a minimum width of 60 metres along North Park Drive for a minimum distance of not more or less than 145 metres east of Torbram Road.

The City of Brampton is currently preparing a Comprehensive Zoning By-law. Under the new proposed zoning framework, the subject site is zoned General Employment (GE). At this time, the draft By-law remains under review and is not in-effect.

Surrounding areas:

- North: Employment and industrial uses along North Park Drive, including warehouse uses.
- > East: Various Industrial uses, including a vehicle assembly plant.
- > South: Variety of employment and commercial buildings
- ➤ West: Residential subdivision consisting of one and two-storey semi-detached and detached residential dwellings.

DESCRIPTION OF PROPOSAL

The latest development concept for the subject site proposes two one-storey industrial buildings with associated surface parking. Total Gross Floor Area (GFA) for the concept is 63,939 m²; Building 1 has a GFA of 33,237 m² and Building 2 has a GFA of 30,702 m². Both industrial buildings accommodate truck level doors along their eastern faces and a total of 100 trailer parking spaces are provided. Vehicular parking is provided within surface parking areas that are separate from the truck parking area to the north, south, and west of the proposed buildings. A total of 381 vehicular parking spaces are currently included as part of the concept. Access from North Park Drive and Williams Parkway is to be provided via un-signalized limited moves intersections; a signalized, all-moves intersection along Torbram Road in alignment with Jardine Street to the west, is being proposed as part of this concept. This access on Torbram Road would be for typical vehicles only, and not truck trailer movements. The placeholder locations two transformers on site conform with zoning requirements as they are located behind yard setbacks. Final location is to be confirmed by Alectra Utilities.

POLICY CONTEXT AND PLANNING ANALYSIS

Variance Requested

The Zoning Chart below outlines the proposed variances to the existing zoning by-law.

| Variance Number | Zoning Requirement | Zoning Requirement | Proposed | Section of the section of |
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| 1 | North Park Drive Setback | 25 metres | 24 metres |
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| 2 | Parking Spaces | Building 1 INDUSTRIAL - WAREHOUSE 168 SPACES (FOR FIRST 20,000 m² + 13,109/170 (78 SPACES) = 246 Building 2 INDUSTRIAL - WAREHOUSE 168 SPACES (FOR FIRST 20,000 m² + 8,125/170 (48 SPACES)= 231 Total Required: 477 | Building 1: 174 Building 2: 207 Total: 381 |
| 3 | Height | 10.8 m | 14.31 m |
| 4 | Outdoor Storage | (2) outside storage of good, materials and equipment is permitted, provided that such storage areas are located no closer than: (i) 150.0 metres to Williams Parkway; (ii) 90.0 metres to Torbram Road; (iii) 25.0 metres to North Park Drive; and, (iv) 180.0 metres to Airport Road. | |

Landscape

Buffers

5

metres along North Park Drive minimum distance. for a minimum distance of not more or less than 145 metres east of Torbram Road

To permit a landscape buffer (3) A landscape strip of 9 m having a minimum width of 60 along North Park Drive with no

Planning Act

Pursuant to Section 45(1) of the Planning Act, R.S.O. 1990, c. P.13 ("the Act"), The Committee of Adjustment can approve minor variances to Zoning By-laws for land, buildings, or structures upon the Owner's request, if they believe the changes are in keeping with the general purpose and intent of the Official Plan and Zoning By-law; are desirable for the appropriate development or use of the land, building or structure; and are minor in nature (the "Four Tests"). The variances must also be consistent with the Provincial Planning Statement, 2024 ("PPS").

Provincial Planning Statement (2024)

In accordance with Section 5 of the Planning Act, all decisions must be consistent with policies found within policy statements passed under Section 3 of the Act. The Provincial Planning Statement ("PPS") (2024) is a policy document passed under Section 3, and therefore decisions must be consistent with the policies therein. The PPS provides provincial direction on matters related to land use planning including growth, housing supply, economic development, and the protection of natural and cultural heritage resources.

FOUR TESTS ANALYSIS

It is TBG's professional planning opinion that the proposed variances satisfy the four tests stipulated by The Planning Act as follows:

The requested variances meet the four tests stipulated by The Planning Act as outlined below:

1) Is the general intent and purpose of the Official Plan maintained?

The Region of Peel Official Plan ("RPOP") provides policy direction for sustainable development that weaves together environment, social, economic, and cultural priorities. The Subject Site is designated as *Urban System* and *Employment Areas* within the RPOP. Section 5.8 provides policy direction for *Employment Areas*. *Employment Areas* are identified in the RPOP as lands that are to be protected for a range of employment uses and encourage more intensive use of land close to current and planned transit routes. Policy direction within the RPOP is supportive of intensification of existing Employment Areas with compatible employment uses (Policy 5.8.23).

The City of Brampton Official Plan ("OP") provides policy direction for the strategic planning for the rapid growth anticipated in the City. The Subject Site is designated as *Employment* within the OP. The OP encourages *Employment Areas* to be preserved for growth and intensification (Policy 2.2.8.1.a). Policy stipulates that employment areas should be planned and designed to be easily accessible via a range of transportation modes (Policy 2.1.2.36). Policy also stipulates that a diverse range of job opportunities should be accommodated within proximity to major goods movement infrastructure including Pearson Airport (Policy 2.1.2.33). Torbram Road and Williams Parkway are also identified as *Support Corridors* as per schedule 1A of the OP. Policy encourages infill opportunities within this area and buildings that are taller in height, up to 4 storeys (low rise plus) within 800 m of the *Support Corridor* (Policy 2.2.5.14).

The Airport Intermodal Secondary Plan Area ("AISPA") provides a land use policy framework for the employment area north of Lester B. Pearson International Airport. The AISPA designates the

site as General Employment 1. The AISPA states that the General Employment 1 designation may permit a broad range of industrial uses including warehousing, and storage of goods, manufacturing, processing, repairing and servicing operations, and outdoor storage area as an accessory to an industrial use.

Variance 1

Variance 1 aligns with the RPOP, OP, and AlSPA as the proposed development supports infill of *Employment Lands* and will contribute to Brampton's future growth and intensification targets for employment.

The proposed setback variance for North Park Drive is required as the in-place zoning for the site was based on the original parcel configuration. Under the current zoning, the required street line setback from Williams Parkway is 30 metres. From that street line, the proposed setback is 57.9 metres. At the closest location of the property line, which is not the street line, the proposed setback is 20.8 metres. Based on the language of the zoning in-place, it is our opinion that a variance for the setback from Williams Parkway is not required. The in-place zoning standards for the Torbram Road and interior side yard setbacks are complied with.

Now that the new development parcel has been created through the consent process, relief from the applicable site specific zoning is required to implement zoning provisions that will facilitate development on the new parcel. As such, it is our opinion that the setback variances requested for North Park Drive conform with the RPOP and OP and that the general intent and purpose is maintained.

Variance 2

The reduction of parking spaces allows for an increased footprint to accommodate growth targets for *Employment Lands*. Additionally, both the OP and the RPOP provide policy direction for the development of Employment uses within proximity to transit. The reduction of parking spaces promotes transit use and encourages carpooling amongst employees. In addition, the reduction in parking area allows for the provision of generous landscape strips around the periphery of the site. As such, it is our opinion that the variance for parking reduction conforms with the RPOP and OP and that the general intent and purpose is maintained.

In addition, a Parking Study has been prepared by TYLin and is enclosed, which provides justification for the reduced parking rate. Proxy sites included 155-161 Orenda Road and 250 Chrysler Drive. TYLin concludes that based on the observed utilization of the proxy sites, and the ITE Parking Generation Manual, a variance to permit the proposed parking supply of 381 parking spaces is appropriate.

Variance 3

While a variance is required for the proposed height of 14.31, the overall height and massing of the site is consistent with the OP's policy direction for height within *Support Corridors*. It is our opinion that the proposed variance for height conforms to the RPOP and OP and that the general intent and purpose is maintained.

Variance 4

The setbacks for outdoor storage are in keeping with the general intent and purpose of Employment land use in the RPOP and OP. The setbacks provide ample distance between the

public realm and the parking areas. Appropriate screening has been incorporated into the site design to provide visual mitigation. A 2.4 metre high fence is provided at the north end of the site, in proximity of North Park Drive, and a second fence also with a height of 2.4 metres is provided at the south end of the site in proximity of Williams Parkway. A third wall with a height of 3.4 metres is provided in proximity of the proposed entrance along Torbram Road, which also provides noise mitigation. As such, the proposed outdoor storage area is appropriately located and screened from view

In addition, the previous berm that was provided for the site prior to the severance will not be maintained. It is our opinion that a berm is not required to provide suitable screening. The setback distance, landscaped buffers, and planting will provide the appropriate visual mitigation to screen the storage areas from view.

It is our opinion that the proposed variance for outdoor storage conforms to the RPOP and OP and that the general intent and purpose is maintained.

Variance 5

The proposed landscaping buffers are in keeping with the general intent of the RPOP and OP as they provide separation from the Right-of-Ways and assist in providing visual mitigation. Through the previous minor variance application for the lands to facilitate the severance, a minimum landscape strip along Torbram Road of 9 metres was approved. The proposed site plan incorporates similar landscaped strips along the Williams and North Park Drive frontages. A Landscape Plan has been prepared by Studio TLA, which provides detail on the 199 trees that are to be planted around the perimeter of the site within the proposed landscape strips. These trees will serve as visual mitigation and will aid in replacing many of the trees that need to be removed from the site to permit the proposed development. It is our opinion that the proposed variance related to the reduction of landscape strips along North Park Drive and Williams Parkway conforms to the Official Plans.

2) Is the general intent and purpose of the Zoning By-law maintained?

The Subject Site is zoned *Industrial (M2-305)* and *Industrial (M4-307)* and Floodplain (F) as per the City of Brampton Comprehensive Zoning By-Law 270-2004. The applicable zoning facilitated development of the full pre-severed lot, namely the Chrysler plant. As mentioned above, it is our recommendation that the M2-305 zone should apply to the entirely of the site as the M4-307 was implemented specifically for the pre-severance lot for landscaped area. As the site has been created via severance, that zone should be removed and the M2-305 zone should apply. Alternatively, all of the provisions to be implemented through this proposed variance and the applicable zoning within M2-305 zone should be transferred to the M4-307 exception.

Variance 1

The current zoning requires a 25-meter setback from North Park Drive. The proposed development contemplates a 24.3-metre setback, and as such a 24-metre variance is being requested. The Zoning By-Law 270-2004 requires buildings in the *M2* parent zone to have a minimum rear lot line of 7 metres. Given that the site is a through lot and corner lot per the City's zoning definition, the shorter of the lot lines is deemed the front lot line, and as such the North Park Drive setback is deemed to be the rear year. Therefore, the proposed variance to permit a setback of 24 metres exceeds what is typically required for industrial sites zoned *M2* within the

City of Brampton. The proposed setback is aligned with the general intent of the Zoning By-law for Industrial uses.

Variance 2

The current zoning requires 168 parking spaces for buildings over 20,000 square metres plus an additional parking space per 170 square metres of gross floor area (GFA). This calculation would require 246 parking spaces for Building A and 231 parking spaces for Building B. The proposed development would provide 174 parking spaces for Building A and 207 parking spaces for Building B, for a supply of 381. As noted above, a Parking Study has been prepared which demonstrates that the proposal will provide an appropriate supply of parking to meet demand. As such, the reduced parking is aligned with the general intent of the Zoning By-law and provides an appropriate amount of parking for the subject use of the Site.

Variance 3

The current zoning stipulates that the maximum building height is 10.8 metres. The proposed development contemplates a height of 14.31 metres. Zoning By-Law 270-2004 does not prescribe a limit in metres, however, heights are limited to two storeys on sites that abut a Residential zone. Both proposed buildings will be one storey in height. As a result, the height of the proposed development is aligned with the general intent of the Zoning By-law.

Variance 4

The current zoning stipulates that outdoor storage must be setback 150 metres to Williams Parkway, 90 metres to Torbram Road, and 25 metres to North Park Drive. The proposed development contemplates an outodoor storage setback of 20 m from Williams Parkway and 40 metres from Torbram Road. As described above, the proposal incorporates multiple visual barriers to screen the outdoor storage area from the view of the public realm. In addition, the *M2* parent zone does not establish minimum distances of outdoor storage areas from property lines and only required that storage be screened and located within rear or interior side yards. In addition, there is no requirement of a berm to provide for visual mitigation within the parent zoning. As such, the proposed distances between outdoor storage and property lines, and the proposed removal of the existing berm meets the general intent and purpose of the Zoning By-law.

Variance 5

The current zoning stipulates that there should be a minimum landscape buffer of 30.0 metres along Williams Parkway, a 9 metre strip along Torbram Road, and a 60 metre strip along North Park Drive with a distance of 145 metres. The proposed development proposes a 3.5 metre strip of landscaping along Williams Parkway, a 9 metre landscape strip along North Park Drive, and a 9 metre landscape strip along Torbram, which is in compliance with the previous Minor Variance approval for the site. Required minimum landscaping strips under the M2 parent zone are 3 metres abutting a street or industrial use, and 6 metres abutting any Residential zone. As such, the proposed landscape strips meet the general intent and purpose of the Zoning By-law as the proposed landscape strips exceed what is required within the parent M2 zone. In addition the reduced landscape strips are suitable as they are still sized in order to facilitate substantial deciduous and coniferous tree plantings to provide ample visual mitigation.

3) Are the proposed variances appropriate for the development of the land and/or building?

The proposed variances are appropriate for the development of the land. The zoning for this land was initially established for the land's use as an automotive assembly plant. In 2024, excess lands belonging to the owner of the assembly plant were severed through a consent, and are now proposed to be used for two new industrial buildings as described above. The reduced setbacks for the building and outdoor storage, reduction of parking, increased height, and reduction in landscape buffers are all appropriate for the development of the land for industrial use and are appropriate for the proposed building type. The zoning standards contemplated through this application are consistent with the zoning standards that are implemented for other warehouse type uses throughout the City of Brampton. In addition, several of the proposed standards exceed the minimum requirements within the *M2* parent zone, as noted above.

For the reasons outlined in this letter, it is our professional planning opinion that the proposed Minor Variance is appropriate for the development of the land and building.

4) Are the proposed variances minor?

While the magnitude of the zoning relief required may not appear 'minor' it is important to review the proposed variances in the context of the current zoning which was implemented for presevered lot. Looking at the nature of the variances in the context of the newly created development site, the variances are minor in nature as they conform to the general intent of the Official plan, Zoning By-law, facilitate suitable development of two new industrial buildings within an industrial land use and do not negatively impact the public. The reduced setbacks (Variances 1) will permit the infilling of employment uses, ensuring more efficient land use and economic activity within the City.

Variance 2 will reduce the number of parking spaces; this reduction is minor and will be offset by proximity to transit routes. In addition, a detailed Parking Study has been prepared by TyLin supporting the reduction through review of proxy sites.

Variance 3 will increase the maximum height by 3.51 metres. In our opinion, the height variance is justified by the generous building setbacks and landscaping that will provide screening and minimize the visual impact of the buildings.

Variance 4 contemplates the decreased storage setbacks, appropriate screening and setbacks are being provided to mitigate the impact of the outdoor storage.

Finally, Variance 5 contemplates reduced landscape buffers, while the landscape buffers would be reduced through the proposed development, the landscape plan includes trees and shrubbery that will mitigate the visual impact of the site minimizing the effect of the reduced landscaped buffers.

The site has been designed to have negligible impacts on the immediate neighbouring lands and the public realm. Care has been taken to ensure that the proposed development will ensure a suitable massing and scale of development, as well as a comfortable public realm.

For the reasons outlined in this letter, it is our professional opinion that the proposed Minor Variances are minor in nature.

In addition, the proposed transformer locations will not require variances should the proposed setback for North Park Drive be approved. Neither transformer locations are located within any required yard setback, in compliance with section 6.10 of Zoning By-law 270 – 2004.

CLOSING

It is our professional Planning Opinion that the application meets the four tests of minor variance and should be approved by the Committee of Adjustment.

We trust you will find all in order, however if you have any questions or require additional information, please contact us at your earliest convenience.

Respectfully,

THE BIGLIERI GROUP LTD.

Mallory Nievas, MES, MCIP, RPP

Associate

∟eyľa Mains, MA

Planner

DREAM

9501-9511 TORBRAM ROAD, BRAMPTON

ARCHITECTS PROJECT # 255002

A PROJECT FOR

DREAM

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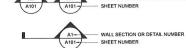
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VICINITY MAP





9501-9511 TORBRAM ROAD

DRAWING INDEX



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| 1020 | | | | 100 | 31.5 | 1000 | | A201-2 | OVERALL BUILDING ELEVATIONS - BUILDING 1 |
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powers brown archit ecture

411 Richmond Street E Suite 206 Toronto, Ontario M5A 3S5 647.931.9787 www.powersbrown.com

PROJECT TITLE

DREAM

9501-9511 TORBRAM ROAD, BRAMPTON

A PROJECT FOR

DREAM

A ALL MATERIALS ON JOB TO COMPLY WITH FEDERAL AND STATE VOC/AIM REGULATIONS.

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DRAWING INDEX, SYMBOL **LEGEND & ABBREVIATION** INDEX

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| ITEM | ONTARIO BUILDING CODE DATA MATRIX PART 3 | | | | | | | | | | | ONTARIO ONTARIO BUILDING CODE 2025 REFERENCES ARE TO DIVISION B UNLESS NOTED (A) FOR DIVISION A OR (C) FOR DIVISION C | | | |
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| 1 | Project Descript | ion. | | | | | | | | | | | | | PART 3 |
| | | RAM ROAD, BRA | MPTON BI DG 1 | | | | _ | | | | _ | New | | | 1.1.1 |
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| 2 | Occupancy Clas | sification: | | | | ım Hazard Industr | | | | | | | | | 3.1.2.1.(1) |
| | Major Occupanc | ies: | | GROU | P F2 Media | ım Hazard Industr | ial Occ | upancy | | | | | | | Table 3.1.2.1 |
| 3 | Building Area (m | | | Existin | | N/A | Nev | w· | 33237 m2 | | Total: | 33237 m2 | | | [A] 1.4.1.2. |
| | | | | | - | N/A | Nes | | 33237 m2 | | Total: | 33237 m2 | | | [A] 1.4.1.2. |
| 4 | Gross Floor Area | | | Existin | | | 1 | | _ | | I Otal: | 33237 mz | | | |
| 5 | Number of Store | ys: | | Above | Grade: | 1 | Bel | ow Grade: | 0 | | | | | | [A] 1.4.1.2. |
| 5b | Building Height (| m): | | 14.31 n | n | | | | | | | | | | 3.2.1.1 |
| 6 | Number of Street | ts Building is Faci | na: | 3 | | | | | | | | | | | 3.2.2.10. |
| 6b | | ighter Access Lar | | 4 | | | | | | | | | | | 3.2.5.4 |
| 7 | Building Classifi | | - | GROU | P F. DIVIS | SION 2, ANY HEIG | HT. A | Y AREA. SP | RINKLERED | | | | | | 3.2.2.57 |
| | | | | - Citto | Entire B | luilding | , | | | | | | | | 3.2.2.18 |
| 8 | Sprinkler System | Proposed: | | | | d Compartments | | | | | | | | | |
| | | | | H | | | | | | | | | | | |
| | | | | H | | d Floor Areas | | | | | | | | | |
| | | | | H | Baseme | | | | | | | | | | |
| | | | | 1 1 | Not Rec | uired | - | I | | _ | | | PER MEP RECOMME | NOATION | |
| 9 | Standpipe & Hos | | | | Yes | | | No | | | | | | | 3.2.5.8. |
| 10 | Fire Alarm Requi | | | | Yes | | | No | | | | | PER MEP RECOMME | | 3.2.4. |
| 11 | | pply is Adequate: | | | Yes | | | No | | _ | | | PER MEP RECOMME | NDATION | 3.2.5.7. |
| 12 | High Building: | | | | Yes | | | No | | | | | | | 3.2.6. |
| 13 | Construction Re | strictions: | | | | stible permited | | | ustible Req'd | | | Both | | | 3.2.2.57 |
| 14 | Mezzanine(s): | | | | Yes | | | No | | | anine(s) Area (m2 |) | | | 3.2.1.1.(3)-(8) and 3.2.8 |
| 14b | Interconnected F | loor Space: | | | Yes | | | No | | | | | | | |
| 15 | Occupant Load: | | | *** TH | S PERMIT | APPLICATION IS | | | HELL OF ON | E BUIL | DING. TENENAT I | IT-OUT WILL BE I | N A SEPARATE PERMI | Т | 3.1.17 |
| | | | | | | d Floor: | N/A | | Total: | TBD | | | | | |
| | Based on Table 3.1.17.1 | | | Third F | | N/A | Forth | Elone: | N/A | | | | | | |
| | | | | Timu r | 1001. | 1471 | ronn | 11001. | 1471 | | | | | | - |
| | Detail Calculation | | | | | | | | | | | | | | |
| | | | | | | Occupancy | | Area (m2) | Additional Ar | ea (m2 | | Occupant Load | | | |
| | | | Ground Floor: | Office | | D | m2 | | N/A | | 9.8 | TBD | | | |
| | | | Ground Floor: | Wareh | ouse | F2 | m2 | | N/A | | 28 | TBD | | | |
| | | | | | | | | | | | | | | | |
| | | | | | | | | | | | | | | | |
| | | | | | | | | | | | | | | | |
| 16 | Barrier-Free Des | ian: | | | | Yes | | П | No | | | | | | 3.8 |
| 10 | Explain: | | | | | 1 | | | | | | | | | |
| 17 | Hazardous Substances: | | | | | Vae | T | | No | | | | I | | 3.3.1.2 |
| 17 | | | | | Tes | | - | NO | | | | | | 0.011.21 | |
| 18 | Explain: Required Fire Resistance Rating: | | | | eletanco | Rating of Horizon | tal As | semblies | Listed Des | ian No | or Description | | | | 3.2.2.57 |
| 10 | Required Fire Re | sistance reating. | | Floors | | reating of Horizon | 2H | Jennones | Listed Bes | igii ito | . or Description | | | | |
| | | | | | | | N/A | | - | | | | | | |
| | | | | Roof: | | | | | _ | | | | | | |
| | | | | Mezzai | nine: | | N/A | | | | | | | | 4 |
| | | | | | | Rating of SUPPO | RTING | Assemblies | Listed Des | ign No | . or Description | | | | |
| | | | | Loadb | earing Wa | lls: | 2H | | | | | | | | |
| | | | | Colum | ns: | | | | | | | | | | |
| 19 | Spatial Separation | n - Construction | of Exterior Walls: | | | | | | | | | | | | |
| | | | | | | Permitted Max | Dre | posed Max. | | | Listed Design | Combustible | Comb.Construction | | 3.2.3 |
| | Wall | Area of EBF (m2) | L.D. (m) | L/H or | H/L | % of Openings | % c | f Openings | FRR (Hrs) | | or Description | Construction | nonComb. Cladding | | |
| | North | 1816 | >15 | | | 100 | | | 0 | | | N/A | N/A | YES | |
| | South | 1816 | >15 | | | 100 | T | | 0 | | | N/A | N/A | YES | |
| | East | 3990 | >15 | | | 100 | | | 0 | | | N/A | N/A | YES | |
| | West | 4000 | >15 | | | 100 | 1 | | 0 | | | N/A | N/A | YES | |
| | | | | - | | 144 | + | · · · | + | _ | | | | | |
| 20 | Plumbing Faciliti | es: | | | | Total Occupan | Min | . WC quired | No. of Barr Free WC R | rier- | Max. No. of | Min. No. of Lavarories Reg'd | | | 3.7.2.2; 3.7.2.2.(12); 3.7.2.2.(13);3.7.2.3.; |
| | | | | | | Load | ree | quirea | Free WC R | eq u | Urinai | Lavarones Req u | | | Tables 3.7.2.2.(A) (B) (C); |
| | | | | Male | | - | + | | - | | | | | | 3.8.2.3.; 3.8.3.8. |
| _ | | | | Female | | - | - | | | | | | | | |
| 100000 | | hroom Required: | | *** THIS | | | FOR (| | | : BUILE | JING. TENENAT F | IT-OUT WILL BE IF | A SEPARATE PERMIT | | 3.8.2.3. |
| 20c | Universal Toilet I | | | | | Yes | - | | No | | | | | | 3.7.2.2.(3); 3.8.3.12. |
| 21 | Number and Loc | ation of Exits fron | Floor Areas: | Min. No | o. of Exits | Required - | No. | | vided - Groun | id | Max. Travel Dist Required: | ance to one Exit | | | 3.4.2./3.4.3.2.A |
| | | | | Ground | 100r: | | - | ur: | | | | | | | |
| | | | | X | | | X | | | | XX m | | | | |
| | | | | Min. No | o of Exits | Required - 2nd & | No | of Exits Pro | vided - 2nd & | Typ. | Max. Travel Dist | ance to one Exit | | | |
| | | | | Typ. FI | r: | , | Fir: | | | ** | Required: | | | | |
| | | | | X | | | x | | | | XX m | | | | |
| 22 | Exit Width based | on Occupant Loa | d and Area of Sui | te: | | | | | | | | | | | 3.4.3 |
| 10000 | Min. Exit Width: | , see | | | | | | | | | | | | | |
| | Door Leaf Min. E | -14 145 -141- | | - | | | | | | - | | | | | 1 |
| | | AIL WIGTH: | | - | | | | | | | | | | | 1 |
| | Occupant Load: | | | - | | | | | | | | | | | 1 |
| | T-1-14. | | warn. | vva | | Danes | | | T | | 1 | | T | | |
| | | width of exits REC | | XXXX r | | Doorway Width | | | - | | - | | | | |
| | Total Aggregate width of exits PROVIDED: | | | | nrfi | Doorway Width | Ц., | | 1 | - | L | | | | |

powers brown archit ecture PROJECT TITLE

411 Richmond Street E Suite 206 Toronto, Ontario M5A 3S5 647.931.9787

DREAM

9501-9511 TORBRAM ROAD, BRAMPTON

A PROJECT FOR

DREAM

PROJECT NO: 255002 DRAWN BY: MZ CHECKED BY: CH SHEET NAME

CODE SUMMARY - BLDG 01

SHEET NUMBER ARCHITECIS Z

ARCHITECIS Z

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| ЕМ | ONTARIO BUILDING CODE DATA MATRIX PART 3 | | | | | | | | | | | ONTARIO ONTARIO BUILDING CODE 2025 REFERENCES ARE TO DIVISION B UNLESS NOTED [A] FOR DIVISION A OR [C] FOR DIVISION C | | | | | |
|----|---|----------------------------|--------------------|----------------------------------|---|---------------------------------|-----------------|-----------------|-------------------|-------------------------------|-------------------|---|--------------|-----------------------------|--|--|--|
| 1 | Project Descripti | ion: | | | | | | | | | | | | PART 3 | | | |
| • | | | | T | П | Alteration | T | Addition | | | New | I | - | 1.1.1 | | | |
| _ | | RAM ROAD, BRA | MPTON BLDG 2_ | | | | | Addition | | | New | | | | | | |
| 2 | Occupancy Clas | sification: | | + | | um Hazard Industr | | | | | | | | 3.1.2.1.(1) | | | |
| | Major Occupanc | ies: | | GROUI | P F2 Medi | um Hazard Industr | ial Occupancy | | | | , | | | Table 3.1.2.1 | | | |
| 3 | Building Area (m | 12): | | Existin | g: | N/A | New: | 30703 m2 | | Total: | 30703 m2 | | | [A] 1.4.1.2. | | | |
| 4 | Gross Floor Area | a (m2): | | Existin | a: | N/A | New: | 30703 m2 | | Total: | 30703 m2 | | | [A] 1.4.1.2. | | | |
| 5 | Number of Store | | | | Grade: | 1 | Below Grade: | | | | | | | [A] 1.4.1.2. | | | |
| | | | | 14.31 | Grade. | | Delow Grade. | | | | | | | 3.2.1.1 | | | |
| 5b | Building Height (| | | - | | | | | | | | | | 3.2.2.10. | | | |
| 6 | | ts Building is Fac | | 3 | | | | | | | | | | 3.2.5.4 | | | |
| 5b | | ighter Access La | 10: | | D.F. DN# | SION 2, ANY HEIG | LIT AND ADEA C | מחווועו בחבם | | | | | | 3.2.2.57 | | | |
| 7 | Building Classifi | | | GROU | Entire E | | HI, ANT AREA, S | PRINKLERED | | | | | | 3.2.2.18 | | | |
| 8 | Sprinkler System | n Proposed: | | | Selecte | d Compartments d Floor Areas | | | | | | | | | | | |
| | | | | H | Not Red | | | | | | | | | | | | |
| • | Standpipe & Hos | a Doguirod: | | H | Yes | quireu | ■ No | | T | | | PER MEP RECOMME | NDATION | 3.2.5.8. | | | |
| 9 | Fire Alarm Requi | | | H | Yes | | □ No | | + | | | PER MEP RECOMME | | 3.2.4. | | | |
| 11 | | irea: upply is Adequate | | | Yes | | □ No | | | | | PER MEP RECOMME | | 3.2.5.7. | | | |
| | High Building: | | | I | Yes | | No No | | | | | 1 | | 3.2.6. | | | |
| 13 | Construction Re | strictions. | | 1 1 | | stible permited | | bustible Req'o | 1 | | Both | | | 3.2.2.57 | | | |
| 14 | Mezzanine(s): | ouona. | | Ti | Yes | | No No | | Mezz | anine(s) Area (m2) | | | | 3.2.1.1.(3)-(8) and 3.2.8 | | | |
| 4b | Interconnected F | loor Space: | | - | Yes | | No No | | | , , , , , , , , , , , , , , , | | | | ***** | | | |
| 15 | Occupant Load: | loor opace. | | | | APPLICATION IS | | SHELL OF ON | IE BUILI | DING. TENENAT F | IT-OUT WILL BE IN | A SEPARATE PERMIT | | 3.1.17 | | | |
| | Based on Table 3 | 1.17.1 | | | d Floor: | | Second Floor: | N/A | | Total: | TBD | | | | | | |
| | | | | Th | | Third F | | | Forth Floor: | N/A | | | | | | | |
| - | Detail Calculatio | n T | | T | | Occupancy | Floor Area (m2) | Additional A | roa (m2 |) m2/person | Occupant Load | | | | | | |
| - | | | Ground Floor: | Office | | D | m2 | N/A | rea (mz | 9.8 | TBD | | | | | | |
| | | | | Wareho | niiso | F2 | m2 | N/A | - | 28 | TBD | | | | | | |
| | | | Ground Floor: | Traienc | 3036 | FZ | IIIZ | 14// | | 20 | TBD | | | | | | |
| | | | | - | | - | | - | | | | | | | | | |
| | | | | - | | - | | | | - | | | | | | | |
| 6 | Barrier-Free Des | lan | | - | | Yes | | No | | - | | | | 3.8 | | | |
| 6 | Explain: | iigir. | | - | | 165 | | NO | | | | | | | | | |
| 7 | Hazardous Subs | tances: | | _ | | Yes | | No | | | | | | 3.3.1.2. | | | |
| • | Explain: | tances. | | _ | | 100 | | INO | | - | | | | | | | |
| 8 | Required Fire Re | | Fire-Re | sistance | Rating of Horizon | tal Assemblies | Listed De | sign No | . or Description | | | | 3.2.2.57 | | | | |
| - | | Floors: | | | 2H | | | | | | | | | | | | |
| | | | | Roof: | | | N/A | | | | | | | | | | |
| | | | | Mezzar | alna: | | N/A | | | | | | | | | | |
| | | | | Fire-Resistance Rating of SUPPOR | | | a Listed De | sian No | o. or Description | | | | | | | | |
| | | | | | 2H | Listed De | J.g.: 110 | . or becomplien | | | | | | | | | |
| | | | | Colum | | | - | _ | | | | | | | | | |
| 9 | Spatial Saparatio | on - Construction | of Exterior Welle: | Colum | 110. | | | | | | | | | | | | |
| | Spatial Separation | on - construction | LALERIOI WAIIS. | T | | Permitted Max | Proposed Max | | | Listed Design | Combustible | Comb.Construction | Non-Comb. | 3.2.3 | | | |
| | Wall | Area of EBF (m2) | L.D. (m) | L/H or | нл | % of Openings | % of Openings | | | or Description | Construction | nonComb. Cladding | Construction | 3.2.3 | | | |
| | | 1816 | >15 | | | 100 | | 0 | - | | N/A | N/A | YES | | | | |
| | | 1816 | >15 | | | 100 | | 0 | | | N/A | N/A | YES | | | | |
| | | 3645 | >15 | | | 100 | | 0 | | | N/A | N/A | YES | | | | |
| | | 3754 | >15 | | | 100 | | 0 | | | N/A | N/A | YES | | | | |
| | 11001 | | | † | | Total Occupan | Min. WC | No. of Ba | | Max. No. of | Min. No. of | | | 3.7.2.2; 3.7.2.2.(12); | | | |
| 0 | Plumbing Faciliti | ies: | | 1 | | Load | Required | Free WC | | Urinal | Lavarories Req'd | | | 3.7.2.2.(13);3.7.2.3.; | | | |
| | | | | Male | | | | | - | | | | | Tables 3.7.2.2.(A) (B) (C); | | | |
| | | | | Female | | | | | | | | | | 3.8.2.3.; 3.8.3.8. | | | |
| ь | Barrier-Free Was | shroom Required: | | | | APPLICATION IS | FOR CORE AND | SHELL OF ON | E BUIL | DING. TENENAT F | IT-OUT WILL BE IN | A SEPARATE PERMIT | | 3.8.2.3. | | | |
| c | Universal Toilet I | | | | | | | | | | | | | 3.7.2.2.(3); 3.8.3.12. | | | |
| 1 | | ation of Exits from | n Floor Areas: | Min. No | o. of Exits | Required - | No. of Exits Pr | rovided - Grou | nd | Max. Travel Dist | ance to one Exit | | | 3.4.2./3.4.3.2.A | | | |
| | | | | Ground | floor: | | Floor: | | | Required: | | | | | | | |
| | | | | X | | | x | | | XX m | | | | | | | |
| | | | | | o of Exits | Required - 2nd & | No. of Exits Pr | ovided - 2nd & | TVp. | Max. Travel Dist | ance to one Exit | | | | | | |
| | | | | | Min. No. of Exits Required - 2nd & Typ. Fir: | | Typ. Fi | r: | , | Fir: | | *** | Required: | | | | |
| | | | | | | | × | | | XX m | | | | | | | |
| | | | | X | | | | | | | | | | 3.4.3 | | | |
| | Exit Width based | i on Occupant Los | d and Area of Suit | te: | | | | | | | | | | | | | |
| 2 | Exit Width based Min. Exit Width: | I on Occupant Los | d and Area of Suit | te: | | | | | | | | | | | | | |
| 2 | | | d and Area of Suit | te: | | | | | | | | | | | | | |
| 2 | Min. Exit Width: | | d and Area of Suit | te: | | | | | | | | | | | | | |
| 2 | Min. Exit Width: Door Leaf Min. E Occupant Load: | xit Width: | | | | | | | | | | | | | | | |
| | Min. Exit Width: Door Leaf Min. E Occupant Load: Total Aggregate | | QUIRED: | X te: | | Doorway Width | | | | | | | | | | | |

powers brown archit ecture

411 Richmond Street E Suite 206 Toronto, Ontario M5A 3S5 647.931.9787

PROJECT TITLE

DREAM

9501-9511 TORBRAM ROAD, BRAMPTON

A PROJECT FOR

DREAM

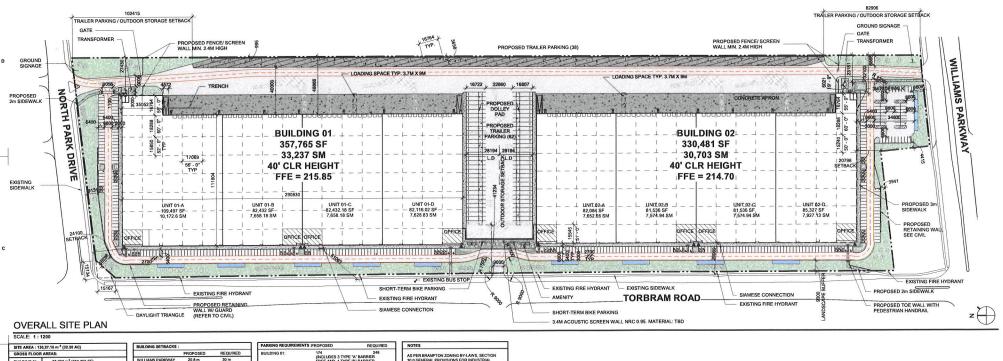
| DATE | REVISION |
|------------|----------------|
| 2025/04/17 | Issued for SPA |
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PROJECT NO: 255002 DRAWN BY: MZ CHECKED BY: CH SHEET NAME

CODE SUMMARY - BLDG 02

SHEET NUMBER ARCHITECTS Z

G002.2



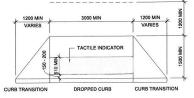
| | SITE AREA: 130,27.10 m ² (32.20 AC) | | | | | | | | | |
|--|--|-------------------------------------|---|--|--|--|--|--|--|--|
| | GROSS FLOOR AREAS: | | | | | | | | | |
| | BUILDING 01 : | 33,237 m ² (357,765 SF) | _ | | | | | | | |
| | BUILDING 02: | 30,702 m ² (330,481 SF) | | | | | | | | |
| | TOTAL: | 63,939 m ² (688, 246 SF) | | | | | | | | |
| | SITE COVERAGE: | 49% | | | | | | | | |
| | BUILDING HEIGHT | 14.31 m (REQUIRED MAX. 10.8 m) | | | | | | | | |
| | | | | | | | | | | |

| | PROPOSED | REQUIRED | BUILDING 01: |
|-------------------|---------------|----------|---------------------------|
| WILLIAMS PARKWAY | 20.8 m | 30 m | Anvertendostarious |
| TORBRAM ROAD | 24.2 m | 9 m | |
| NORTH PARK DRIVE | 24.1 m | 25 m | BUILDING 02: |
| PROPOSED DISTANCE | FOR OUTDOOR S | TORAGE: | DOILDING CA |
| | PROPOSED | REQUIRED | |
| WILLIAMS PARKWAY | 82.9 m | 150 m | TOTAL PARKING: |
| TORBRAM ROAD | 41.4 m | 90 m | TRAILER PARKING: |
| NORTH PARK DRIVE | 102.4 m | 25 m | LOADING DOCKS: |
| AIRPORT ROAD | N.A. | 180 m | |
| | | | SNOW STORAGE 7,788 SF / 7 |
| | | | |

| EMENT | TS :PROPOSED | REQUIRED | NOTES |
|---------|--|-------------|---|
| | 174 (INCLUDES 3 TYPE FREE AND, 4 TYPE FREE, 10CP AND 12 | "B" BARRIER | AS PER BRAMPTON ZONING BY-LAWS, SECTION 30.0 GENERAL PROVISIONS FOR INDUSTRIAL ZONES - ITEM 30.5 (PARKING SPACES - WAREHOUSE): |
| | 207 (INCLUDES 3 TYPE FREE AND, 4 TYPE FREE, 10CP AND12 | "B" BARRIER | OVER 20,000 SQUARE METRES: 168 PARKING SPACES PLUS 1 PARKING SPACE PER 170 SQUARE METRES GROSS FLOOR AREA OR PORTION THEREOF THAT IS OVER 20,000 |
| | 381 | 477 | SQUARE METRES |
| | 100 | | BUILDING 01: |
| | 102 | | 188 SPACES (FOR FIRST 20,000 m ²) + {(13,237/170) = 78 SPACES } = 168+78 = 246 SPACES |
| 7,788 S | F / 723 SM | | BUILDING 02: 168 SPACES (FOR FIRST 20,000 m ² + ((10,703/170) = 63 SPACES) = 168+63 = 231 SPACES |

| AREA OF EACH | :H TENANT SPACE: | | | |
|---------------|---------------------------------------|------------|---------------------------------------|---|
| BUILDING 01 : | | | | |
| UNIT NUMBER | AREA | DOCK DOORS | AREA/ DOCK DOORS | OFFICE AREA |
| UNIT 01 -A | 109,497 SF (10,172.6 m ²) | 09 | 1/12,216 SF (1/1,130 m ²) | 3,441 SF or 319.67 m ² (3.14%) |
| UNIT 01 - B | 82,432 SF (7,658.18 m ²) | 14 | 1/5,888 SF (1/547 m ²) | 2,604 SF or 241.91 m ² (3.15%) |
| UNIT 01- C | 82,432 SF (7,658.18 m ²) | 15 | 1/5,495 SF (1/510 m ²) | 2,604 SF or 241.91 m ² (3.15%) |
| UNIT 01 -D | 82,984 SF (7,709.46 m ²) | 13 | 1/6,383 SF (1/593 m ²) | 2,673 SF or 248.32 m ² (3.18%) |
| TOTAL: | 357,345 SF (33,198 m²) | 51 | 1/6,742 SF (1/603 m²) | 11,322 SF or 1,051 m 2 (3.16%) |

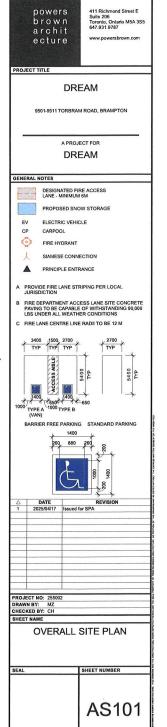
| BUILDING 02 : | | | | |
|---------------|--------------------------------------|------------|------------------------------------|---|
| UNIT NUMBER | AREA | DOCK DOORS | AREA/ DOCK DOORS | OFFICE AREA |
| UNIT 02 -A | 82,082 SF (7,652.66 m ²) | 13 | 1/6,314 SF (1/586 m ²) | 2,587 SF or 240.34 m ² (3.15% |
| UNIT 02 - B | 81,536 SF (7,574.94 m ²) | 15 | 1/5,435 SF (1/504 m ²) | 2,520 SF or 234.11 m ² (3.09% |
| UNIT 02- C | 81,536 SF (7,574.94 m ²) | 13 | 1/6,272 SF (1/582 m ²) | 2,520 SF or 234.11 m ² (3.09% |
| UNIT 02-D | 85,327 SF (7,927.13 m ²) | 10 | 1/8,232 SF (1/764 m ²) | 2,587 SF or 240.34 m ² (3.03%) |
| TOTAL: | 330,481 SF (30,702.6 m 2) | 51 | 1/6,480 SF (1/602 m ²) | 10,214 SF or 948.9 m ² (3.09% |



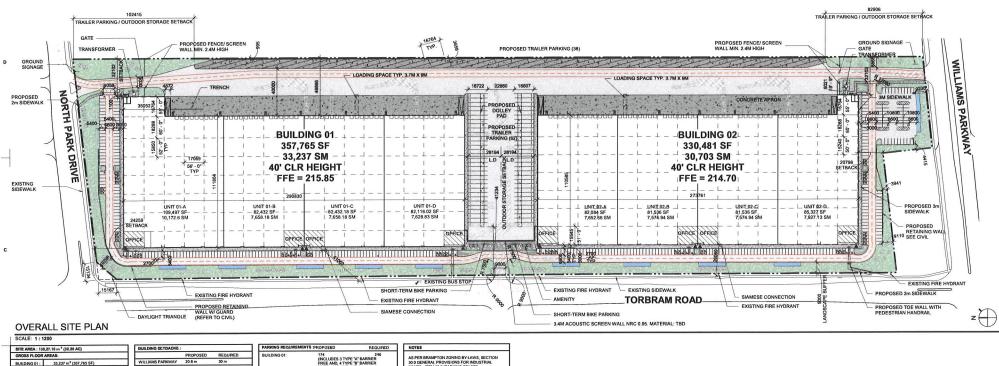
Received / Revised

JUN 11 2025

Committeee of Adjustment



C4 CURB CUT



| SITE AREA: 130,2 | 27.10 m ² (32.20 AC) |
|------------------|------------------------------------|
| GROSS FLOOR A | REAS: |
| BUILDING 01 : | 33,237 m ² (357,765 SF) |
| BUILDING 02: | 30,702 m ² (330,481 SF) |
| TOTAL: | 63,939 m² (688, 246 SF) |
| SITE COVERAGE: | 49% |
| BUILDING HEIGHT | : 14.31 m (REQUIRED MAX. 10.8 m) |

| | PROPOSED | REQUIRED |
|-------------------|---------------|----------|
| WILLIAMS PARKWAY | 20.8 m | 30 m |
| TORBRAM ROAD | 24.2 m | 9 m |
| NORTH PARK DRIVE | 24.3 m | 25 m |
| PROPOSED DISTANCE | FOR OUTDOOR S | TORAGE: |
| | PROPOSED | REQUIRED |
| WILLIAMS PARKWAY | 82.9 m | 150 m |
| TORBRAM ROAD | 41.4 m | 90 m |
| NORTH PARK DRIVE | 102.4 m | 25 m |
| AIRPORT ROAD | N.A. | 180 m |

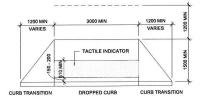
| | PARGONG REQUIREMENT | 2 PROPOSED | REQUIRED |
|----------|---------------------|---|-------------|
| = | BUILDING 01: | 174 (INCLUDES 3 TYPE FREE AND, 4 TYPE FREE, 10CP AND1: | "B" BARRIER |
|) | BUILDING 02: | (INCLUDES 3 TYPE FREE AND, 4 TYPE FREE, 10CP AND 12 | "B" BARRIER |
| \dashv | TOTAL PARKING: 38 | | 477 |
| - | TRAILER PARKING: | 100 | |
| | LOADING DOCKS: | 102 | |

SNOW STORAGE 7,788 SF / 723 SM

| | NOTES |
|----------|---|
| - 1 | AS PER BRAMPTON ZONING BY-LAWS, SECTION |
| - 1 | 30.0 GENERAL PROVISIONS FOR INDUSTRIAL |
| - 1 | ZONES - ITEM 30.5 (PARKING SPACES - |
| _ | WAREHOUSE): |
| - 1 | OVER 20,000 SQUARE METRES: 168 PARKING |
| - 1 | SPACES PLUS 1 PARKING SPACE PER 170 |
| - 1 | SQUARE METRES GROSS FLOOR AREA OR |
| - 1 | PORTION THEREOF THAT IS OVER 20,000 |
| -1 | SQUARE METRES |
| - | BUILDING 01: |
| - 1 | 168 SPACES (FOR FIRST 20,000 m 2) + ((13,237/170) |
| - 1 | = 78 SPACES) = 168+78 = 246 SPACES |
| \dashv | BUILDING 02 : |
| _ | 168 SPACES (FOR FIRST 20,000 m 2 + ((10,703/170) |
| | = 63 SPACES) = 168+63 = 231 SPACES |

| BUILDING 01 : | G 01 : | | | |
|---------------|---------------------------------------|------------|------------------------------------|---|
| UNIT NUMBER | AREA | DOCK DOORS | AREA/ DOCK DOORS | OFFICE AREA |
| UNIT 01 -A | 109,497 SF (10,172.6 m ²) | 09 | 1/12,216 SF (1/1,130 m 2) | 3,441 SF or 319.67 m ² (3.14%) |
| UNIT 01 - B | 82,432 SF (7,658.18 m ²) | 14 | 1/5,888 SF (1/547 m ²) | 2,604 SF or 241.91 m ² (3.15%) |
| UNIT 01- C | 82,432 SF (7,658.18 m ²) | 15 | 1/5,495 SF (1/510 m ²) | 2,604 SF or 241.91 m ² (3.15%) |
| UNIT 01 -D | 82,984 SF (7,709.46 m ²) | 13 | 1/6,383 SF (1/593 m ²) | 2,673 SF or 248.32 m ² (3.18%) |
| TOTAL: | 357,345 SF (33,198 m²) | 51 | 1/6,742 SF (1/603 m²) | 11,322 SF or 1,051 m2 (3.16%) |

| BUILDING 02: | | | | | | |
|--------------|---------------------------------------|------------|------------------------------------|--|--|--|
| UNIT NUMBER | AREA | DOCK DOORS | AREA/ DOCK DOORS | OFFICE AREA | | |
| UNIT 02 -A | 82,082 SF (7,652.66 m ²) | 13 | 1/6,314 SF (1/586 m ²) | 2,587 SF or 240.34 m 2 (3.15% | | |
| UNIT 02 - B | 81,536 SF (7,574.94 m ²) | 15 | 1/5,435 SF (1/504 m ²) | 2,520 SF or 234.11 m ² (3.09% | | |
| UNIT 02-C | 81,536 SF (7,574.94 m ²) | 13 | 1/6,272 SF (1/582 m ²) | 2,520 SF or 234.11 m ² (3.09% | | |
| UNIT 02 -D | 85,327 SF (7,927.13 m ²) | 10 | 1/8,232 SF (1/764 m ²) | 2,587 SF or 240.34 m ² (3.03% | | |
| TOTAL: | 330,481 SF (30,702.6 m ²) | 51 | 1/6,480 SF (1/602 m ²) | 10,214 SF or 948.9 m ² (3.09% | | |





powers brown archit ecture

411 Richmond Street E Suite 206 Toronto, Ontario M5A 3S 647.931.9787 www.powersbrown.com

PROJECT TITLE

DREAM

9501-9511 TORBRAM ROAD, BRAMPTON

A PROJECT FOR

DREAM



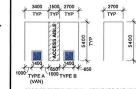
CP CARPOOL

FIRE HYDRANT

SIAMESE CONNECTION

PRINCIPLE ENTRANCE

- A PROVIDE FIRE LANE STRIPING PER LOCAL JURISDICTION
- B FIRE DEPARTMENT ACCESS LANE SITE CONCRETE PAVING TO BE CAPABLE OF WITHSTANDING 90,000 LBS UNDER ALL WEATHER CONDITIONS
- C FIRE LANE CENTRE LINE RADII TO BE 12 M



BARRIER FREE PARKING STANDARD PARKING



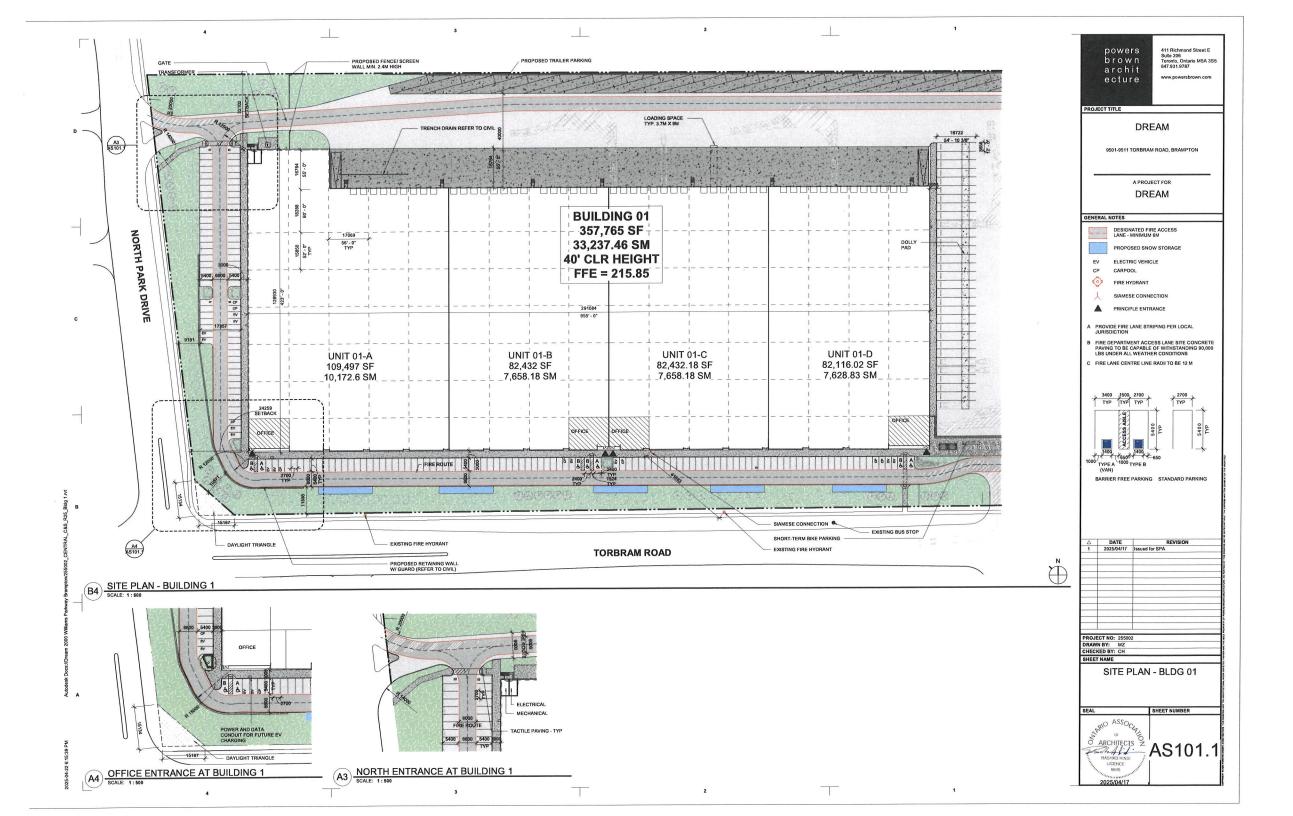
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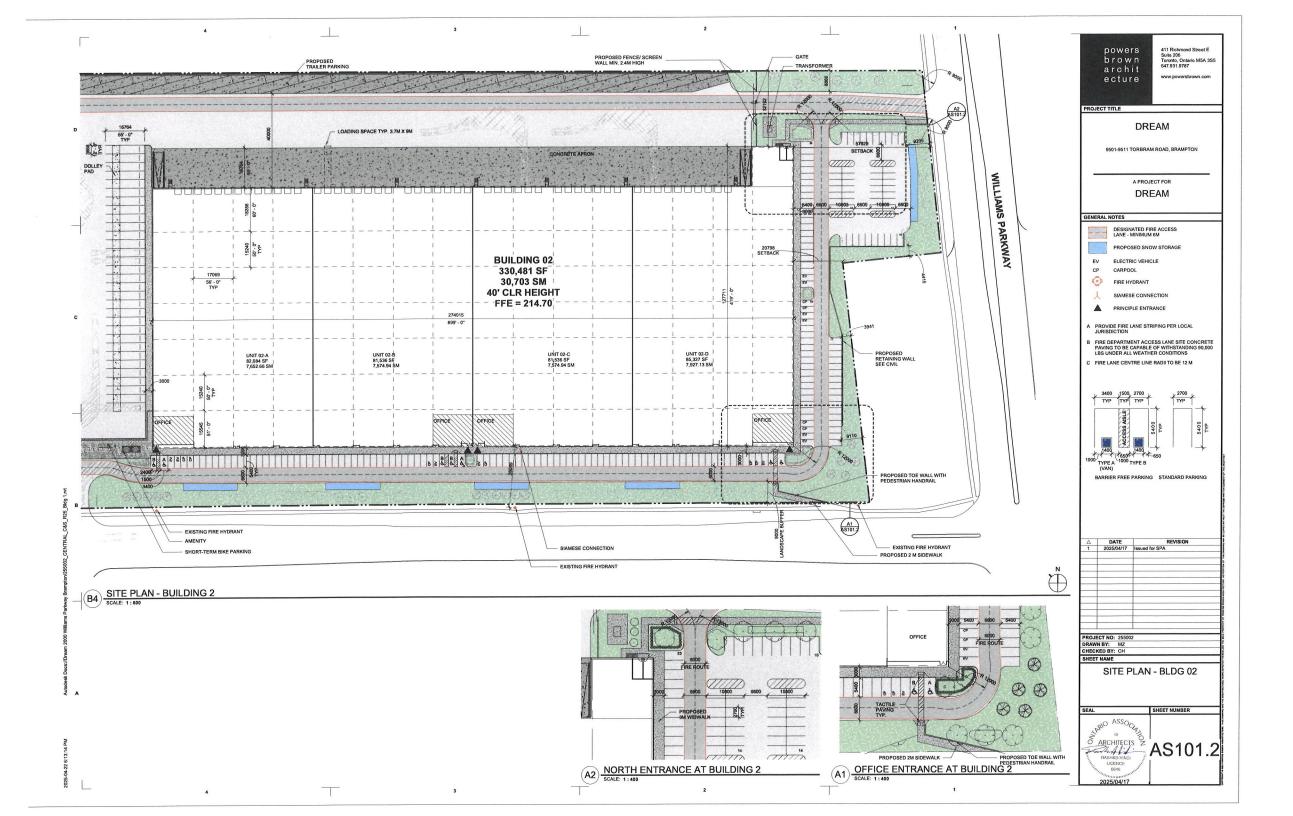
SHEET NAME

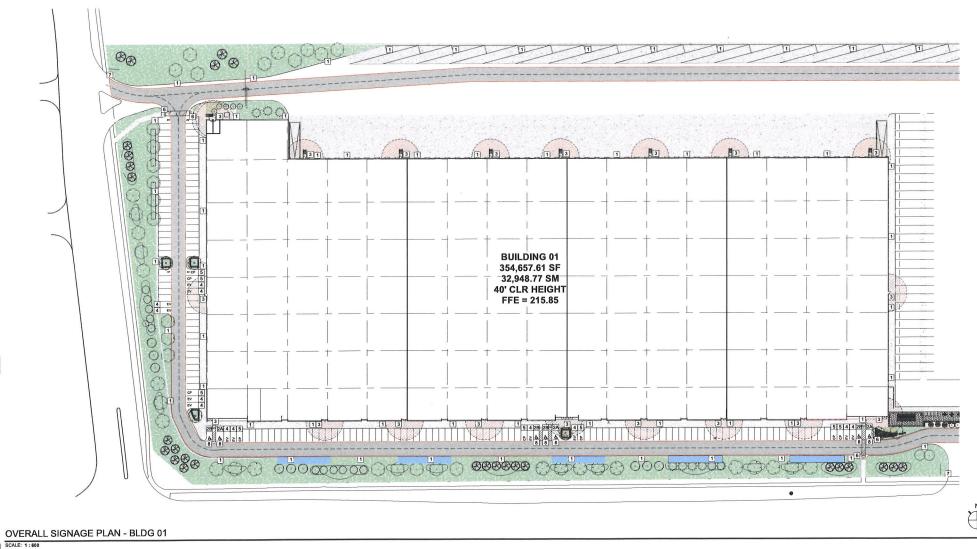
OVERALL SITE PLAN

ARCHITECIS Z

AS101









CAR POOL

PARKING

ONLY

TYPE



STOP FOR PEDESTRIANS

TYPE 6













TYPE 7



SIGNAGE IDENTIFICATION NOTES:
1.) SIGNS SHALL BE MOUNTED ON PERMANENT POSTS, POLES, OR BUILDING STRUCTURES.

2.) SIGNS SHALL BE ERECTED AT A HEIGHT OF BETWEEN 1.9M (6 FEET) AND 2.5M (8 FEET) AS MEASURED FROM THE EDGE OF THE TRAVELED PORTION OF THE DESIGNATED ROUTE TO THE BOTTOM EDGE OF THE PROPOSED SIGN.

3.) SIGNS SHALL BE INSTALLED AT A DISTANCE OF BETWEEN 0.3M (1 FOOT) AND 3M (10 FEET) FROM THE TRAVELED EDGE OF THE DESIGNATED ROUTE.

4.) SIGNS AS DESIGNATED SHALL BE INSTALLED ALONG THE ROUTE AT APPROXIMATELY 30M (100 FEET) INTERVALS OR AS FREQUENTLY AS IS NECESSARY TO IDENTIFY THE ROUTE IN JUDGEMENT OF THE CHIEF FIRE OFFICIAL.

5.) SIGNS SHALL BE INSTALLED SO THAT THERE ARE NO OBSTRUCTIONS WITHIN 6M (20 FEET) OF THE SIGN AND BE VISIBLE TO APPROACHING

6.) SIGNS SHALL DISPLAY SINGLE HEADED ARROWS POINTING IN THE DIRECTION IN WHICH THE REGULATION IS IN EFFECT AT INTERMEDIATE POINTS THROUGHOUT THE REGULATED AREA, DOUBLE HEADED ARROWS SHALL NOT INDICATE THE PROHIBITION OR RESTRICTION EXISTS IN EACH DIRECTION.

- 7.) BYLAW NUMBERS SHALL NOT BE INDICATED ON A FIRE ROUTE SIGN.
- 8.) A DESIGNATED FIRE ACCESS ROUTE AND SIGNAGE SHALL BE MAINTAINED BY THE PROPERTY OWNER.

9.) A FINAL FIRE DEPARTMENT INSPECTION WILL BE REQUIRED AS PART OF THE FINAL APPROVAL. TO ARRANGE FOR A FINAL INSPECTION, PLEASE CONTACT THE FIRE PREVENTION & PUBLIC SAFETY DIVISION AT 416-338-9450 (WEST COMMAND).

brown archit ecture Toronto, Ontario M5A 3S5 647.931.9787

PROJECT TITLE

DREAM

A PROJECT FOR

DREAM



DESIGNATED FIRE ACCESS LANE - MINIMUM 6M





ELECTRIC VEHICLE

© FIRE HYDRANT

SIAMESE CONNECTION

PROVIDE FIRE LANE STRIPING PER LOCAL JURISDICTION

FIRE DEPARTMENT ACCESS LANE SITE CONCRETE PAVING TO BE CAPABLE OF WITHSTANDING 90,000 LBS UNDER ALL WEATHER CONDITIONS

NO PARKING - FIRE ROUTE DIRECTION SIGN (SPACED EVERY 30M OR LESS)

2A ACCESSIBLE PARKING BY PERMIT ONLY SIGN -TYPE A - VAN ACCESSIBLE 2B ACCESSIBLE PARKING BY PERMIT ONLY SIGN - TYPE B

NO SMOKING WITHIN 25' OF ENTRANCES SIGN TYP @ ALL ENTRANCES/EXITS ELECTRIC VEHICLE PARKING/CHARGING SIGN

CARPOOL PARKING ONLY SIGN

STOP FOR PEDESTRIANS / PEDESTRIAN WALKWAY SIGN - TYP ALL MARKED PEDESTRIAN CROSSINGS

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SHEET NAME

SIGNAGE PLAN -BLDG 01

SHEET NUMBER ² AS102.1

BUILDING 02 4 EV 4 EV 5 C 5 C 4 EV 334,080.56 SF 31,037.10 SM **40' CLR HEIGHT** FFE = 214.7088

OVERALL SIGNAGE PLAN - BLDG 02

SCALE: 1:600



CAR POOL

PARKING

ONLY

TYPE 5



TYPE



















SIGNAGE IDENTIFICATION NOTES:

1.) SIGNS SHALL BE MOUNTED ON PERMANENT POSTS, POLES, OR BUILDING STRUCTURES.

2.) SIGNS SHALL BE ERECTED AT A HEIGHT OF BETWEEN 1.9M (6 FEET) AND 2.5M (8 FEET) AS MEASURED FROM THE EDGE OF THE TRAVELED PORTION OF THE DESIGNATED ROUTE TO THE BOTTOM EDGE OF THE PROPOSED SIGN.

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powers brown archit ecture Suite 206 Toronto, Ontario M5A 3S5 647.931.9787

PROJECT TITLE

DREAM

9501-9511 TORBRAM ROAD, BRAMPTON

A PROJECT FOR DREAM

GENERAL NOTES





25' RADIUS FOR NO SMOKING ZONE



ELECTRIC VEHICLE



SIAMESE CONNECTION

PROVIDE FIRE LANE STRIPING PER LOCAL JURISDICTION

FIRE DEPARTMENT ACCESS LANE SITE CONCRETE PAVING TO BE CAPABLE OF WITHSTANDING 90,000 LBS UNDER ALL WEATHER CONDITIONS

FIRE LANE CENTRE LINE RADII TO BE 12 M

NO PARKING - FIRE ROUTE DIRECTION SIGN (SPACED EVERY 30M OR LESS)

ACCESSIBLE PARKING BY PERMIT ONLY SIGN TYPE A - VAN ACCESSIBLE 2B ACCESSIBLE PARKING BY PERMIT ONLY SIGN - TYPE B

NO SMOKING WITHIN 25' OF ENTRANCES SIGN-TYP @ ALL ENTRANCES/EXITS

4 ELECTRIC VEHICLE PARKING/CHARGING SIGN

5 CARPOOL PARKING ONLY SIGN

6 STOP FOR PEDESTRIANS / PEDESTRIAN WALKWAY SIGN - TYP ALL MARKED PEDESTRIAN CROSSINGS

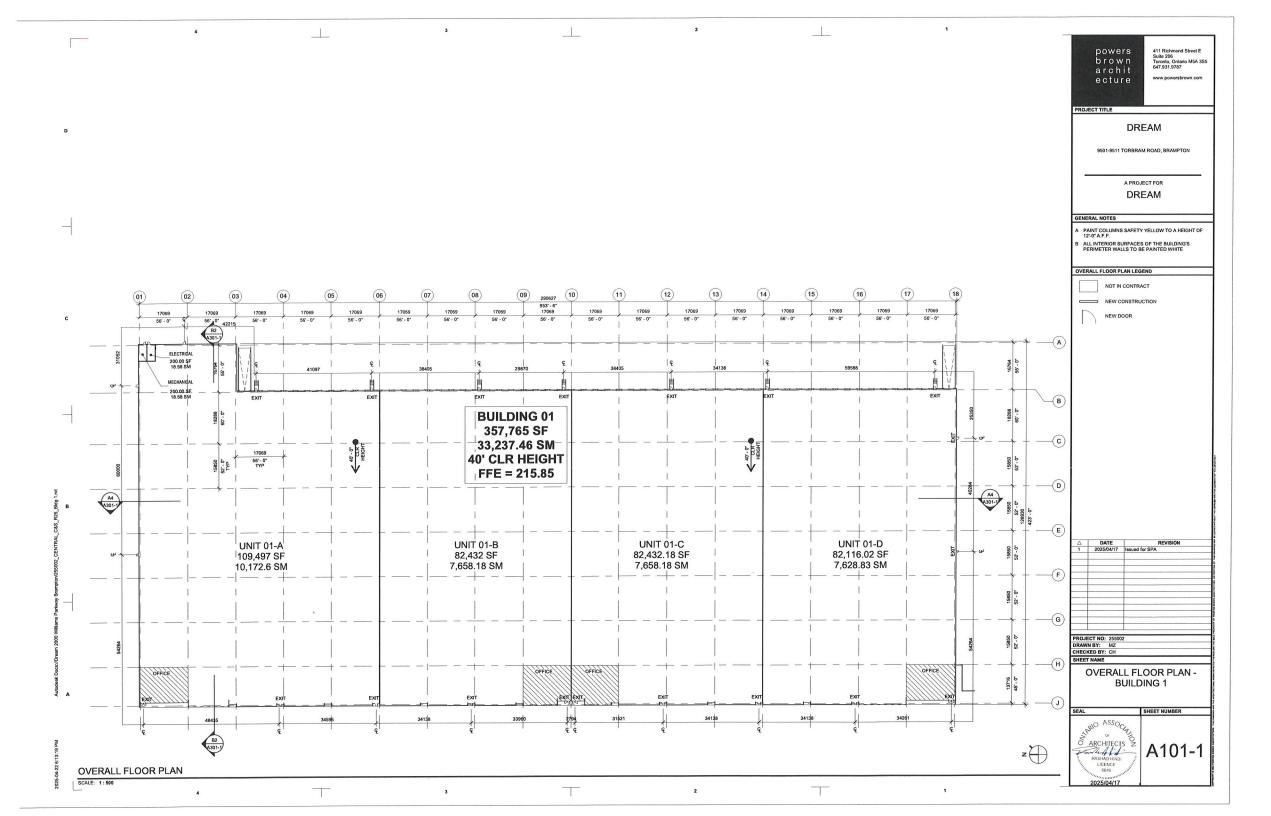
7 INTERSECTION TRAFFIC STOP SIGN

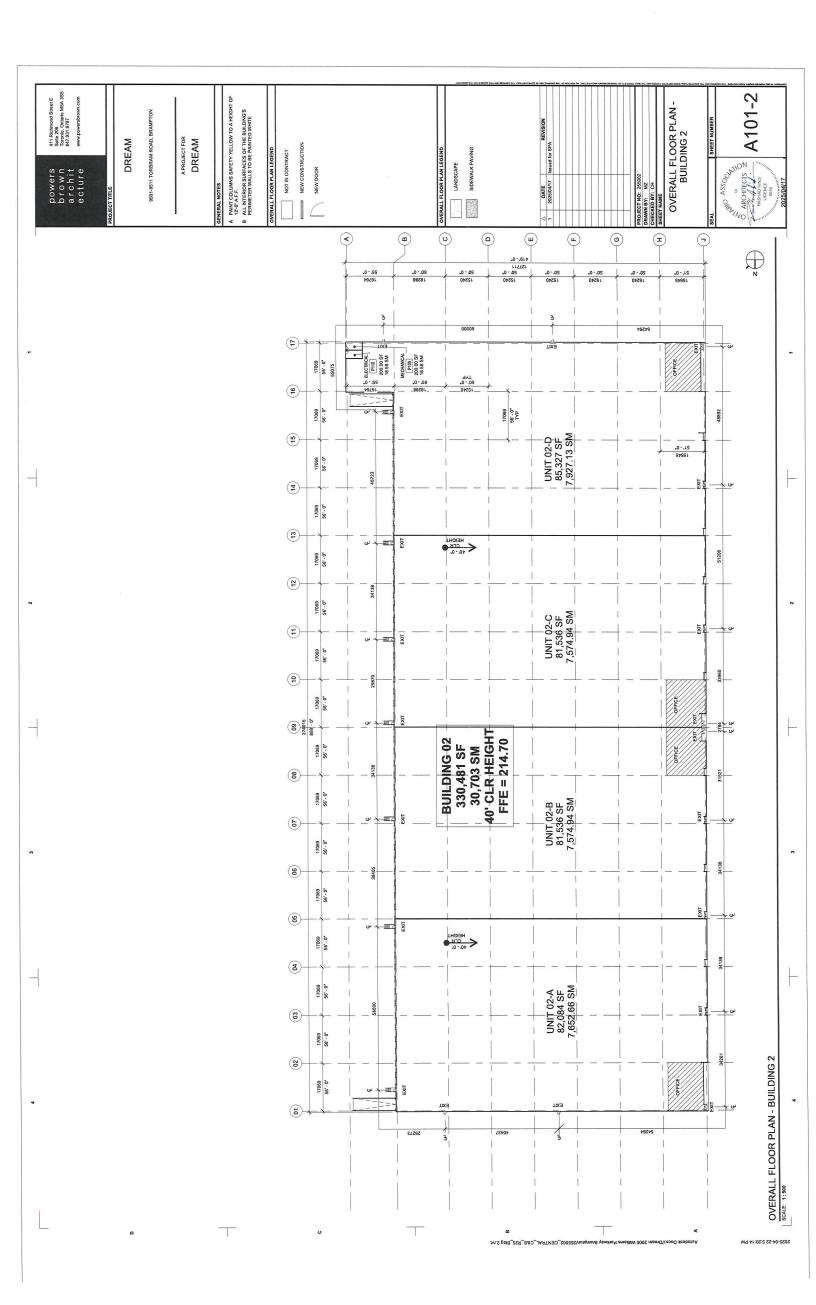
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SHEET NAME

SIGNAGE PLAN -BLDG 02

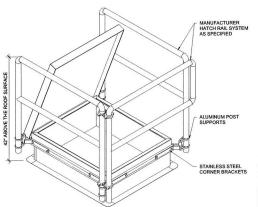
SHEET NUMBER ARCHITECTS AS102.2





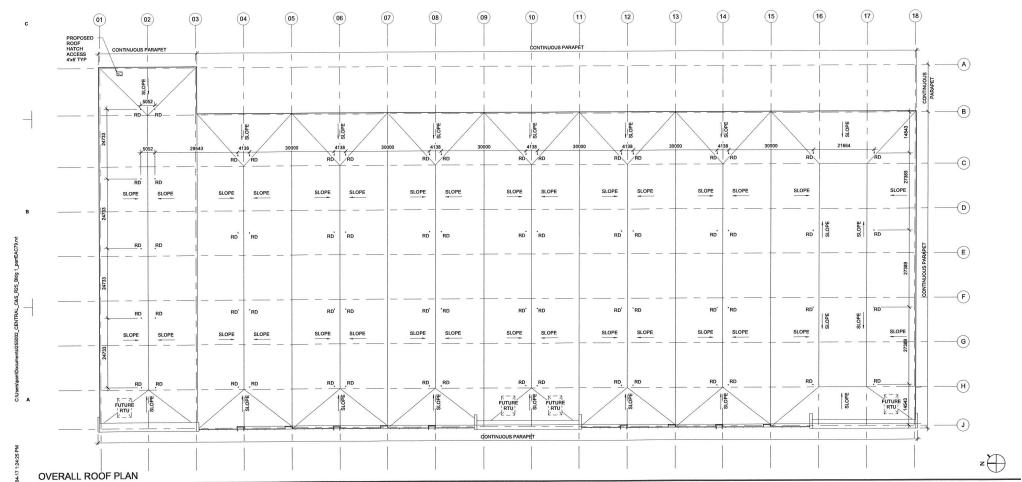
SINGLE - PLY WHITE TPO ROOF (R40MIN) - 100% OF THE ROOF SPACE TPO MEMBRANE TO COMPULES WITH REQUIREMENTS OF A NINIMUM SOLAR REFLECT NIDEX (SRI) OF 78 FOR INDUSTRIAL BUILDINGS AS PER COOL ROOF ALTERNATIVE TO THE GREEN REQUIREMENTS FOR INDUSTRIAL BUILDINGS.

SCALE: 1:500



(D1) ROOF HATCH - TYP

2



3

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powers brown archit ecture 411 Richmond Street E Suite 206 Toronto, Ontario M5A 3S5 647.931.9787

www.powersbrown.com

PROJECT TITLE

DREAM

9501-9511 TORBRAM ROAD, BRAMPTON

A PROJECT FOR

DREAM

GENERAL NOTES

- A INSTALL CONTINUOUS RIGID INSULATION OVER META ROOF DECK RXX
- PROVIDE ROOF CRICKETS FOR POSITIVE DRAINAGE AT ALL ROOF TOP MECHANICAL EQUIPMENT

- PROVIDE WALKWAY PADS FROM ROOF ACCESS
 HATCH TO MECHANICAL EQUIPMENT

 I GO TO ENSURE COMPLIANCE FOR EDGE
 SECUREMENT FOR LOW-SLOPE ROOFS WITH
 REFERENCE STANDARD ANSWSPRI ES-1-03. UNDERSIDE OF ROOF DECK TO BE FACTORY FINISHED WHITE
- PROVIDE OVERFLOW DRAINAGE SCUPPER AT ALL INTERNAL ROOF DRAINS ANDIOR CONDUCTOR HEADS. REFER TO ELEVATIONS AND DETAILS FOR DIMENSIONS AND EXACT LOCATIONS

| - | | _ | | |
|---------|------|----|-----|------|
| OVERALL | POOF | DI | ANI | EGEN |

PREFINISHED METAL CANOPY BELOW

TAPERED INSULATION - SLOPE 1/2 /FT MIN

STEEL CROSS SLOPE 1/4" / FT MIN

ROOF DRAIN TOP TOP OF PARAPET

△ DATE REVISION
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PROJECT NO: 255002 DRAWN BY: MZ CHECKED BY: CH SHEET NAME

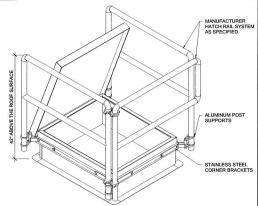
> OVERALL ROOF PLAN -**BUILDING 1**

RIO ASSOC ARCHITECTS PASHADURA

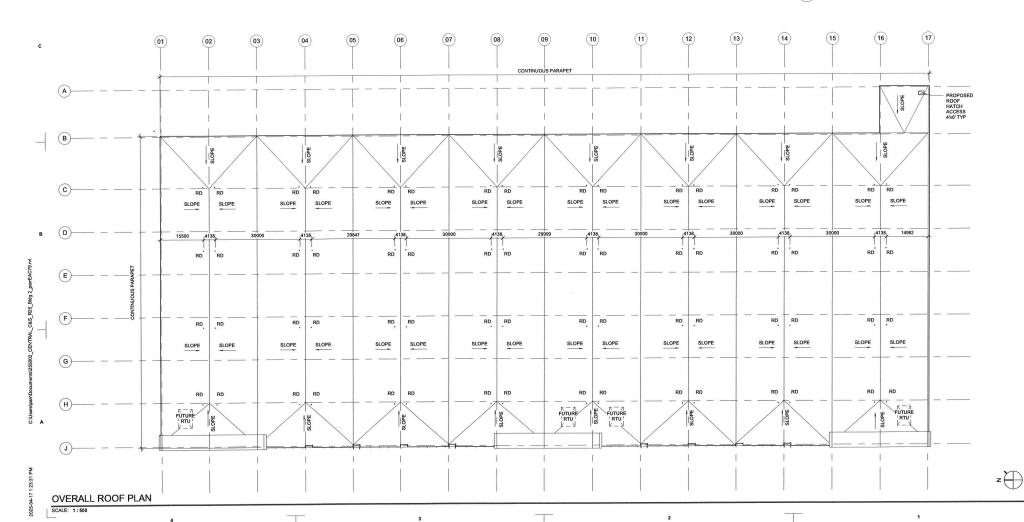
A110-1

SHEET NUMBER

SINGLE - PLY WHITE TPO ROOF (R40MIN) - 100% OF THE ROOF SPACE TPO MEMBRANE TO COMPLES WITH REQUIREMENTS OF A MINIMUM SOLAR REFLECT INDEX (SRI) OF 79 FOR INDUSTRIAL BUILDINGS (AS PER COOL ROOF ALTERNATIVE TO THE GREEN REQUIREMENTS FOR INDUSTRIAL BUILDINGS.



D1) ROOF HATCH - TYP



3

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PROJECT TITLE

DREAM

9501-9511 TORBRAM ROAD, BRAMPTON

A PROJECT FOR

DREAM

GENERAL NOTES

- A INSTALL CONTINUOUS RIGID INSULATION OVER METAL ROOF DECK RXX
- PROVIDE ROOF CRICKETS FOR POSITIVE DRAINAGE AT ALL ROOF TOP MECHANICAL EQUIPMENT

- C PROVIDE WALKWAY PADS FROM ROOF ACCESS
 HATCH TO MECHANICAL EQUIPMENT
 O GC TO ENSURE COMPLIANCE FOR EDGE
 SECUREMENT FOR LOW-SLOPE ROOFS WITH
 REFERENCE STANDARD ANS/SPRI ES-1-03.
- UNDERSIDE OF ROOF DECK TO BE FACTORY FINISHED WHITE
- PROVIDE OVERFLOW DRAINAGE SCUPPER AT ALL INTERNAL ROOF DRAINS ANDIOR CONDUCTOR HEADS REFER TO ELEVATIONS AND DETAILS FOR DIMENSIONS AND EXACT LOCATIONS

| OVERALL | |
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| | PREFINISHED METAL CANOPY | BE |
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TAPERED INSULATION - SLOPE 1/2 /FT MIN

STEEL CROSS SLOPE 1/4" / FT MIN

RD ROOF DRAIN TOP OF PARAPET

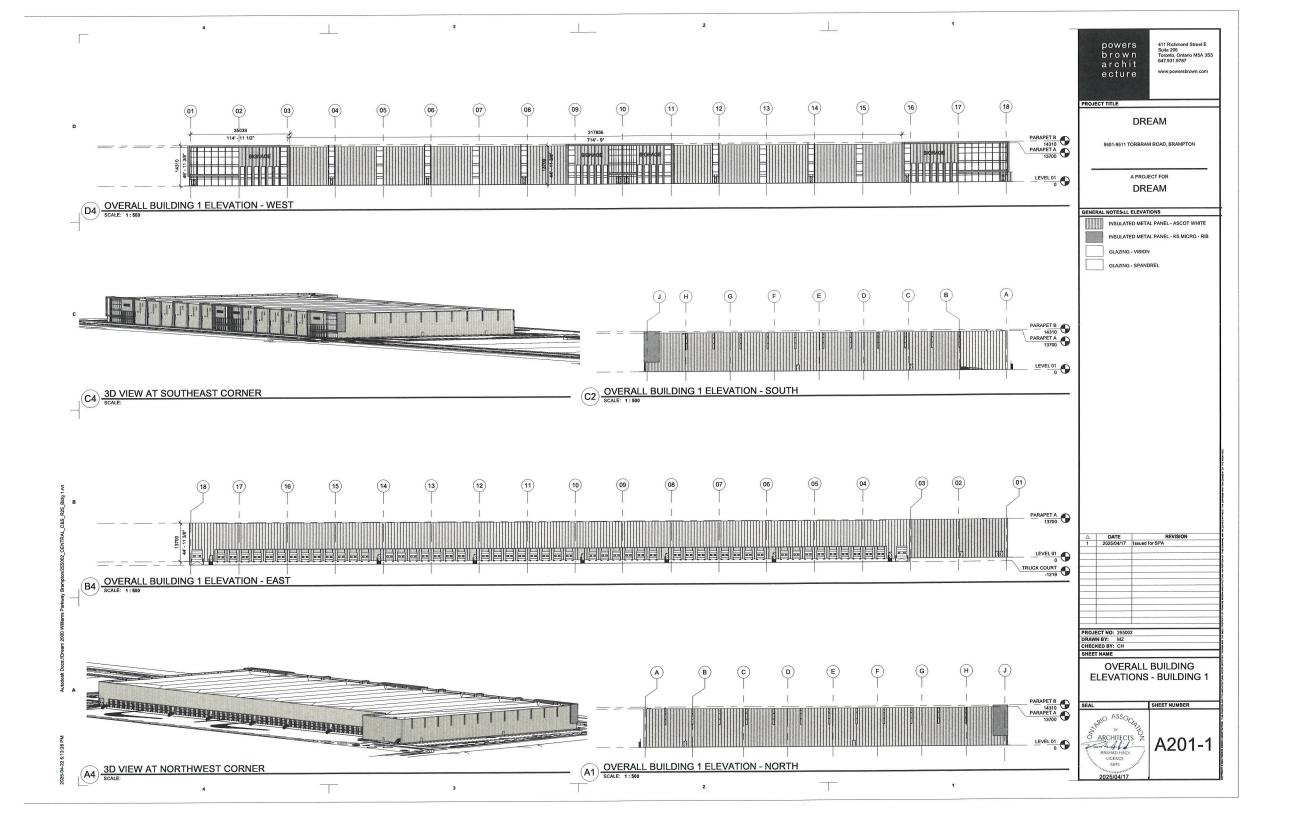
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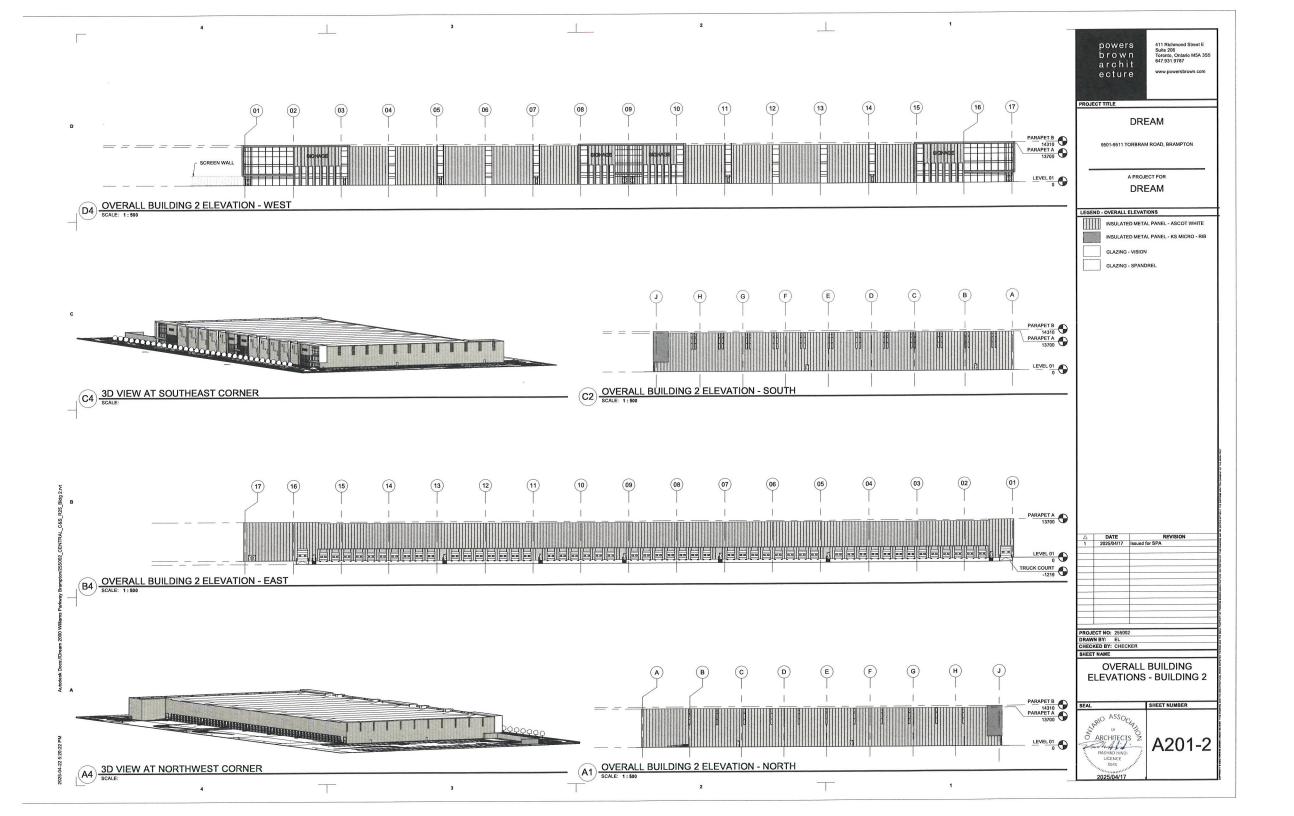
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> OVERALL ROOF PLAN -**BUILDING 2**

SHEET NUMBER BRIO ASSOC ARCHITECTS RASHAD HIN LICENCE 6645

A110-2







KINSPAN IMP ACCENT FIN. COLOUR TO MATCH ADJACENT IMP PANELS

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411 Richmond Street E Suite 206 Toronto, Ontario M5A 3S5 647.931.9787

PROJECT TITLE

DREAM

9501-9511 TORBRAM ROAD, BRAMPTON

A PROJECT FOR

DREAM

Ascot White SR:0.69 E:0.85 SRI:83

2



3.4M ACOUSTIC SCREEN WALL (NRC 0.95)
MATERIAL: TBD

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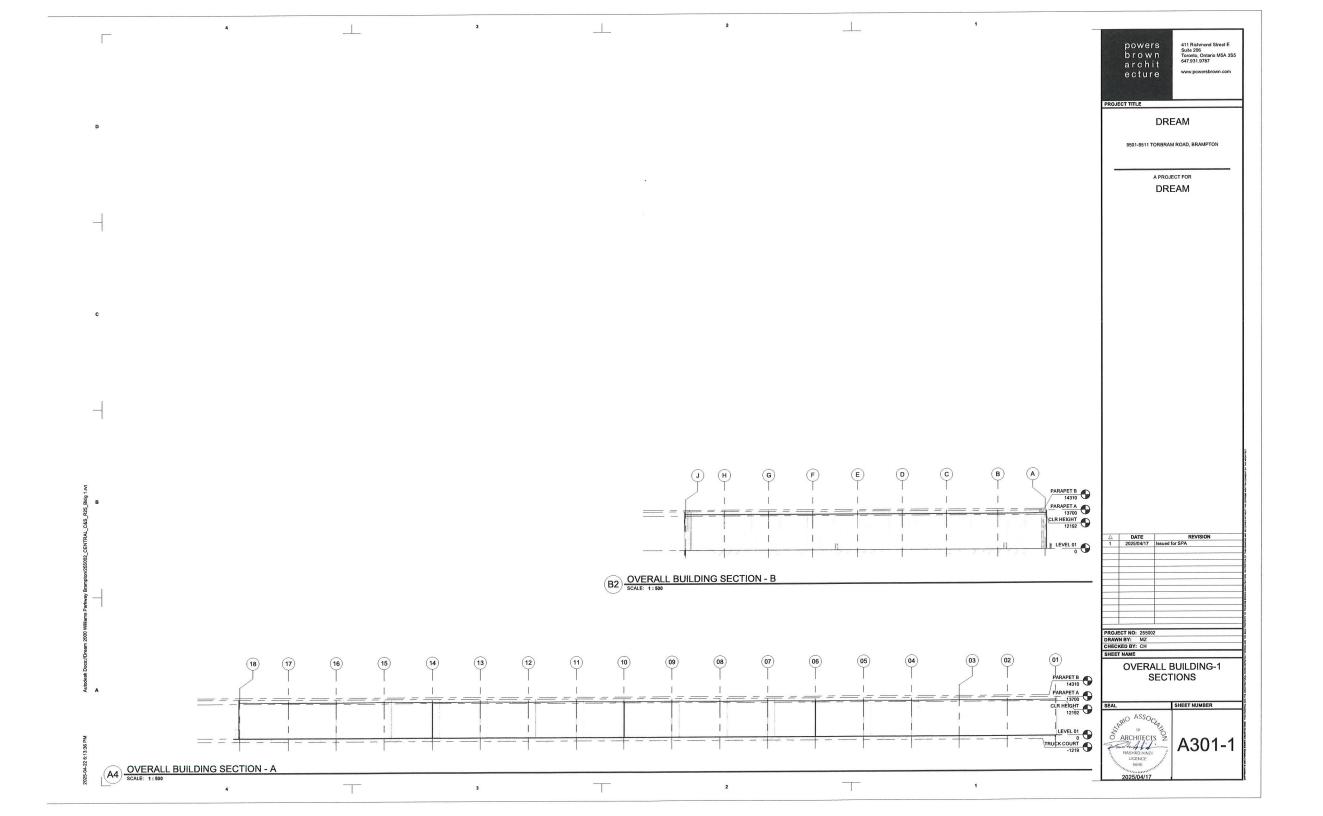
PROPOSED MATERIALS

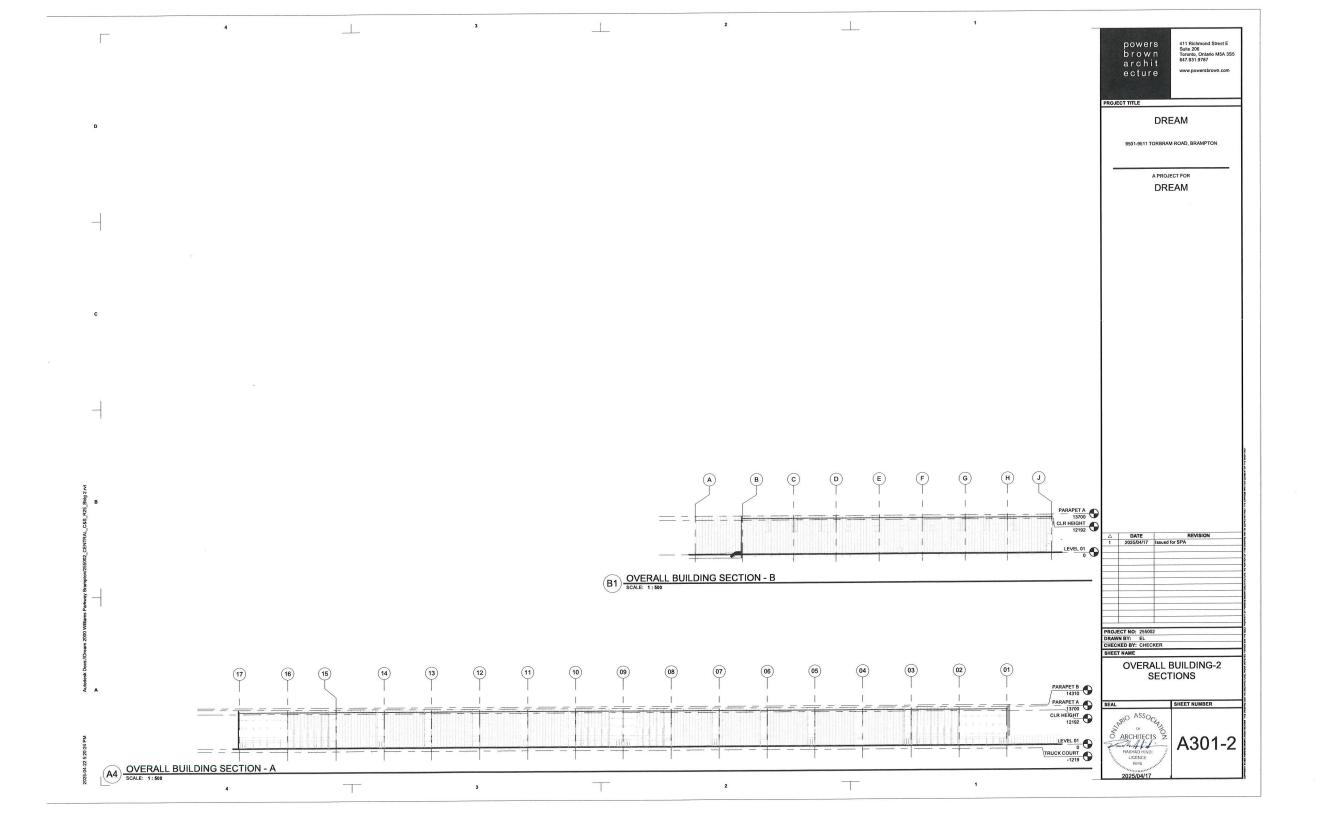
ASSOC ARCHITECTS

RASHAD HINDI
LICENCE
6646

A202

SHEET NUMBER





9501-9511 TORBRAM ROAD

INDUSTRIAL DEVELOPMENT

A PROJECT OF DREAM

PROJECT ISSUANCE

LANDSCAPE ARCHITECT



20 Champlain Boulevard, Suite 102 Toronto, ON M3H 2Z1

CONSULTANTS

POWERS BROWN ARCHITECTURE 260 KING ST E, SUITE A501 TORONTO, ON M5A 4L5

HUSSON 200 CACHET WOODS COURT, SUITE 204 MARKHAM, ON L6C 0Z8

STAMP



REVISIONS

1 ISSUED FOR SITE PLAN APPROVAL 2025-04-17

PHASE SPA PHASE PROJECT NUMBER PROJECT 25-102



Sheet List Table

Sheet Number Sheet Title

LC.000 COVERSHEET

LS.100 OVERALL LANDSCAPE PLAN PLANTING SCHEDULE LS.101

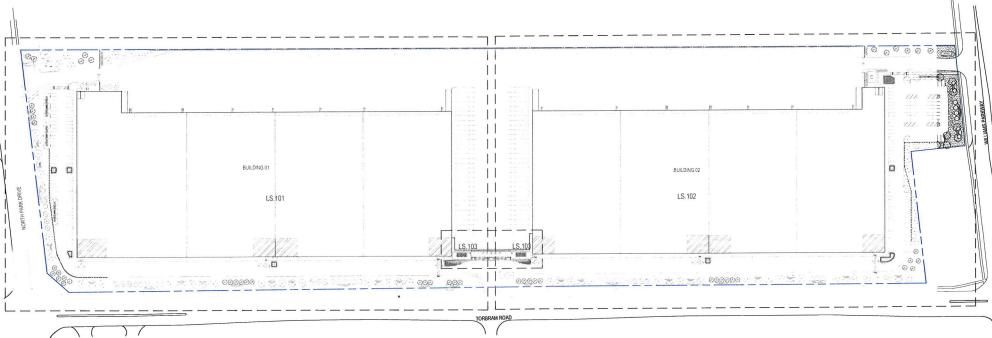
LP.101 LANDSCAPE PLAN

LANDSCAPE PLAN LP.102

LANDSCAPE ENLARGEMENT PLAN LP.103

LANDSCAPE DETAILS LD.100

LANDSCAPE DETAILS



GENERAL NOTES:

- CONTRACTOR SHALL OBTAIN ALL NECESSARY PROVINCIAL OR LOCAL MUNICIPAL PERMITS REQUIRED, AL CONSTRUCTION SHALL CONFORM TO PROVINCIAL AND LOCAL MUNICIPAL STANDARDS AND CODES THAT PERTAIN TO THE SITE LUNDER CONSTRUCTION.
- CONTRACTOR SHALL ARRANGE FOR UTILITY STAKING PRIOR TO START OF CONSTRUCTION, INCLUDING ANY TEST PIT EXCAVATIONS TO VERIFY EXACT LOCATIONS.
- 3. IT IS THE CONTRACTORS RESPONSBULTY TO WIST THE STE PRIOR TO CONSTRUCTION TO BECOME FAMILIAR WITH EXISTING CONDITIONS. IF ANY DISCREPANCES EXIST BETWEEN THE DRAWNIGS AND ACTUAL SITE CONDITIONS, CONTRACTOR SHALL BRING THIS TO THE ATTENTION OF THE LANDSCAPE ARCHITECT.
- CONTRACTOR SHALL PROVIDE HIS OWN LAYOUT, GRADING, STAKING, AND SURVEYING REQUIRED FOR CONSTRUCTION. REFER TO EXISTING SURVEY FOR BENCHWARKS AND OTHER EXISTING INFORMATION. CONTRACTOR SHALL PIELD VERIFY LAYOUT PRIOR TO CONSTRUCTION.
- EXCAVATION IN THE WCINITY OF UTILITIES SHALL BE UNDERTAKEN WITH CARE. CONTRACTOR BEARS FULL RESPONSIBILITY FOR THE PROTECTION OF ALL UTILITIES.
- CONTRACTOR SHALL PROTECT ALL EXISTING AND NEW CONSTRUCTION FROM DAMAGE. SHOULD ANY DAMAGE OCCUR, CONTRACTOR SHALL MAKE ALL NECESSARY REPAIRS 1 TO PUT EXISTING ELEMENTS BACK TO THERE EXISTING CONDITION BEFORE DAMAGE OCCURED, AT NO COST TO THE OWNER.
- EXSING BASE INFORMATION INCLUDING UNDERBROADURUILITIES SHOWN ON THE DRAWNOS HAVE BEEN PROVIDED FROM A TOPICORAPHIC SURVEY OR DIGITAL MAY OWNERS GROUP (MIDCI) DOTAL MAPPING FROM THE TIME BEEN BASE OF THE STATE OF
- ALL DRAWINGS TO BE READ IN CONJUNCTION WITH PROJECT SPECIFICATIONS AND ANY RELATED DOCUMENTS FROM OTHER PROJECT CONSULTANTS.

- 11. KEEP AREA OUTSIDE CONSTRUCTION ZONE CLEAN AND USABLE BY OTHERS AT ALL TIMES.
- CONTRACTOR SHALL SUBMIT SAMPLES OF ALL PROPOSED MATERIALS FOR REVIEW PRIOR TO PLACING ORDERS.
- 13. ALL WORK SHALL BE REVIEWED BY LANDSCAPE ARCHITECT PRIOR TO PAYMENT APPROVAL
- CONTRACTOR SHALL PROVIDE MARKED-UP REDLINE PLANS SHOWING AS-BUILT SITE CONDITIONS TO THE LANDSCAPE ARCHITECT, PRIOR TO OBTAINING APPROVAL OF SUBSTANTIAL COMPLETION.

GENERAL PLANTING NOTES:

- ALL PLANTING AND INSTALLATION IN RIGHT OF WAY SHALL WEET LOCAL MUNICIPAL STANDARDS AND SPECIFICATIONS
- F ANY DISCREPANCY EXISTS BETWEEN THE QUANTITIES, SIZES OR MATERIALS INDICATED ON THE PLAN AND SHOWN IN THE PLANT LIST, THE PLAN SHALL GOVERN.
- IT IS THE LANDSCAPE CONTRACTORS RESPONSIBILITY TO VISIT THE SITE PRIOR TO START OF WORK, TO BECOME FAMILIAR WITH EXISTING CONDITIONS AT THE SITE.
- . VERIFY EXACT LOCATIONS OF ALL UNDERGROUND UTILITIES IN FIELD, REPORT ANY CONFLICTS TO LANDSCAPE ARCHITECT PRIOR TO COMMENCING WORK.
- LANDSCAPE CONTRACTOR SHALL COORDINATE HIS WORK WITH OTHER CONTRACTORS ON SITE TO MINIMIZE DAMAGE TO COMPLETED
 LANN AND PLANT MATERIAL INSTALLATION.
- CONTRACTOR IS RESPONSIBLE FOR SPREADING TOPSOIL AND PLANTING SOIL, FINE CRADING AND PREPARATION OF ALL LAWN AND LANDSCAPE AREAS.
- LANDSCAPE CONTRACTOR SHALL PROVIDE SOIL TEST AWALYSIS FOR ALL SOIL USED ON PROJECT. COPIES OF SOIL AWALYSIS SHALL
 BE PROVIDED TO LANDSCAPE ARCHITECT PRIOR TO ANY LANDSCAPE CONSTRUCTION.
- PRIOR TO SPREADING TOPSOIL, LANDSCAPE CONTRACTOR SHALL INSPECT AND ACCEPT ALL BASE GRADES. ANY DEVIATION FROM LINE AND GRADE INDICATED ON THE GRADING PLAN SHALL BE CORRECTED BEFORE PLACING ANY TOPSOIL.
- BEFORE PLANT MATERIAL INSTALLATION BEGNS, STAKE LOCATIONS OF NEW PLANT MATERIAL AND NOTIFY LANDSCAPE ARCHITECT FOR REVIEW. NOTIFY LANDSCAPE ARCHITECT ONE WEEK MINIMUM PRIOR TO TENTATIVE DATE OF STAKING.
- Contractor shall notify landscape architect if areas of poor drainage or other unusual subsurface conditions are encountered during excavation for planting pits.
- . ALL GRADING, UTILITY, AND IRRIGATION WORK SHALL BE COMPLETED PRIOR TO INSTALLATION OF PLANT MATERIAL AND LANDSCAPE MULCH.
- 12. FINISH GRADES (TOP OF MULCH) SHALL BE 35mm BELOW FINISH PAVING SURFACE IN ALL PLANTING AREAS.
- FINISH SURFACE CRADES OF ALL TURF AREAS SHALL BE 12mm BELOW ADJACENT FINISH PAYING SURFACE, UNLESS OTHERWISE NOTED.
- . CONTRACTOR SHALL BE RESPONSBLE FOR FINE GRADING OF ALL PLANTING AREAS, REMOVAL OF WISCELLANEOUS DEBRIS AND ANY ADDITIONAL FILL REQUIRED TO ACHIEVE A SMOOTH GRADE PRIOR TO PLANTING FINAL GRADING SHALL BE REVEWED BY THE LANGSQUE RACHIECT PRIOR TO PLANTING.
- 15. CONTRACTOR SHALL ENSURE ALL LANDSCAPE AREAS REMAIN CLEAR OF LITTER AND WEEDS DURING CONSTRUCTION
- 16. ALL SHRUBS AND CONFEROUS PLANTINGS SHALL BE INSTALLED IN CONTINUOUS PLANTING BEDS.
- ALL SHRUB BEDS ADJACENT TO LAWN AREAS SHALL HAVE A SPACED EDGE BORDER, UNLESS METAL EDGE BORDER IS SPECIFIED BORDER SHALL BE QUIT 75mm DEEP AT A 45" ANGLE, SO THERE IS A CLEAR AND WELL DEFINED SEPARATION BETWEEN THE PRANTING EDGE AND SODOLO AREA.
- 18. ALL SHRUB BED AREAS SHALL HAVE A MINIMUM 75mm THICK LAYER OF LANDSCAPE MULCH.
- 19. PRE-EMERGENT HERBICIDE SHALL BE USED UNDER MULCH IN ALL LARGE BED AREAS.
- 20. CONTRACTOR SHALL SEED ALL AREAS DISTURBED BY CONSTRUCTION NOT DESIGNATED TO BE SCODED.
- 21. WHERE PROJECT SITE IS TO BE SODDED, CONTRACTOR SHALL PROVIDE NEW TOPSOIL AND SOO IN ANY ADJACENT RIGHT OF WAY FROM PROPERTY LINE TO BACK OF MUNICIPAL. CURB.
- Contractor shall work over lawn areas that have revained partially intact, top dressing with sol, scarfying, and seemns to form a smooth, full, even lawn, free of bare spots, indentations, and weeds.
- 24. WATERIO TO BE PROVIDED FOR A MINIMAN OF THE FIRST 2 CONSCIUTIVE YEARS.

 PAINTING SDEDULE TO BE COORDINATED WITH REGICTION CONTRACTOR TO INSIGHE APPROPRIATE MATERING OF PLANTING AREAS
 AFTER RISTLANDIA LUNGSCAPE CONTRACTOR TO CONSTRUCT PLANTING WITH REGICTION CONTRACTOR TO MINIMAZE CONFLICTS
 BETTER THANTS AND SPRINGER FEAUS AND LINES.

 WEEKLY MATERIO CONTRACTOR.

WATER SHOULD BE APPLIED AT A RATE OF LESS THAN 2-3 CALLONS OF WATER PER MINUTE. CARE SHOULD BE TAKEN NOT TO WASH AWAY ANY WATERNO BERWS OR WILCH WHEN WATERING, TREES NEED 5 CALLONS OF WATER PLUS 5 CALLONS FOR EACH WICH CALIFER ANDREE TO THE FOLLOWING RATIOS

CITY OF BRAMPTON STANDARD LANDSCAPE NOTES L. THE CONTRACTOR MUST MOTIFY THE OPEN SPACE DEVELOPMENT SECTION OF THE CITY OF BRAMPTON PRIOR TO THE COMMENCEMENT OF ANY PLANTING.

- II. THE LOCATIONS OF ALL TREES ON STREET FRONTAGES MUST BE APPROVED BY THE OPEN SPACE DEVELOPMENT SECTION OF THE CITY OF BRANFTON PRIOR TO THEIR INSTALLATION.
- II. THE OWNER IS REQUIRED, UPON COMPLETION OF ALL LANDSCAPE WORKS, TO SUBMIT AN ACCEPTANCE CERTIFICATE PREPARED BY CERTIFIED MAD REQUIRED ONLY LANDSCAPE ARCHITECT TO THE OPEN SPACE DEVELOPMENT SECTION AND TO REQUEST AN INSPECTION OF THE OPEN SPACE SECTION.
- IV. ALL LANGSCAPE WORKS MIL EE CUARANTEED FOR CHE YEAR FOLDWING INSPECTION, PANT MATERIAL, INICH IS NOT IN HEARINY CROWNING CORDITION ONE YEAR AFTER INSPECTION, SHALL BY REPLACED TO THE CHIN'S PRAMETOR WITH AN ADDITIONAL ONE—TERM MAINTENANCE CUARANTEE PERIOD. SPPRY AND PLANT ALL REPLACEMENTS IN STRICT ACCORDANCE WITH PLANS AND STOTE PARCOL.
- SOD THAT IS DAMAGED OR MISSING ON THE PUBLIC BOULEVARD IS TO BE REPAIRED AT THE OWNER'S EXPENSE.
- N. ANY CHAN LINE FOUND AND COMPONENTS THAT ARE INSTALLED SUBSPOLENT TO SITE PLAN APPROVAL SHALL HAVE A BLADS COSS DIMILET PRISH BY POWERS COAT APPLICATION, PRISH TO APPLICATION FANCE! WELL WITH PARKET BOOKREE AND OLGOGIFIEME SOLERSY PAPELS IN A THEORESS OF 4-5 MLS BY ELECTRICASTAIC COAT AND OWN CARED FOR A SMOOTH EVEN SUBFACE. ALL CHANG LINE FRENCE OR BE BLADD VAIL COATION.
- VII. THE CONTRACTOR IS RESPONSBLE FOR LOCATION OF ALL UNDERGROUND SERVICES PROR TO EXCAVATION OF TIREE PITS AND SHRUB BEDS.
- VIII. ALL T-BARDS TO BE REMOVED AT THE CONCLUSION OF THE WARRANTY PERIOD, UNLESS OTHERWISE SPECIFIED BY THE OPEN SPACE SECTION OF THE CITY OF BRAMPTON.
- IX. ANY TRANSFORMER INSTALLED SUBSCOURT TO SITE PLAN APPROVAL SHALL BE SCREENED WITH PLANT MATERIAL TO THE SATISFACTION OF THE CITY OF BRANPTON AND MEET ALL REQUIREMENTS OF HYDRO ONE BRANPTON PLANTING SETBACKS.
- X. ANY DAMAGE DUE TO CONSTRUCTION IX I REQUIRED TO BE REINSTATED AT THE APPLICANT'S EXPENSE. ALL DAMAGES WILL BE RECIFIED TO THE SATISFACTION OF THE OTY OF BRAINFTON.

Landscape Architect

STUDIO II a 20 Champlain Blvd. Suite 102 - Toronto ON - M3H 2Z1 (416) 638-4911 - info@studiotla ca - studiotla ca

9501-9511 **TORBRAM ROAD**

9501-9511 TORBRAM ROAD BRAMPTON, ONTARIO

Project Number 25-102

Client

DREAM 30 ADELAIDE STREET E. SUITE 301 TORONTO, ON M5C 3H1

Consultant

HUSSON CIVIL ENGINEERING 200 CACHET WOODS COURT, SUITE 204 MAYKHAM, ON L6C 028 TEL: 905.709.5625

POWERS BROWN ARCHITECTURE 260 KING STE, SUITE A501 TORONTO, ON M5A 4L5 TEL: 647.931.9787

Revisions

ISSUED FOR SITE PLAN APPROVAL (1)

Stamp



Keymap



Scale

Phase SPA PHASE North

Drawing Name

Sheet Number NOT FOR CONSTRUCTION

OVERALL LANDSCAPE PLAN

LS.100

| PLANT | SCHE | DULE | SITE | | | | | | |
|-----------------|----------|---------|--|-----------------------------------|--------|-----------|--------|---------|-------------------|
| SYMBOL | CODE | QTY | BOTANICAL NAME | COMMON NAME | CONT. | CAL. | HT. | | REMARKS |
| DECIDUOU | JS TREES | 6 | | | | | | , | |
| \odot | AFAB | 8 | ACER FREEMANII 'AUTUMN BLAZE' | AUTUMN BLAZE MAPLE | B & B | 70MM CAL | | | |
| \odot | AR | 5 | ACER RUBRUM | RED MAPLE | B & B | 70MM CAL | NATIVE | | NATIVE |
| \odot | АМСМ | 12 | AMELANCHIER CANADENSIS | MULTI-TRUNK SHADBLOW SERVICEBERRY | В&В | 4M HT | | | 3 CLUMP NATIVE |
| 0 | GTSM | 15 | GLEDITSIA TRIACANTHOS INERMIS 'SHADEMASTER' TM | SHADEMASTER LOCUST | B & B | 70MM CAL. | | | |
| \odot | LT | 12 | LIRIODENDRON TULIPIFERA | TULIP TREE | B & B | 70MM CAL | | | NATIVE |
| \odot | PAB | 28 | PLATANUS X ACERIFOLIA | LONDON PLANE TREE | В&В | 70MM CAL | | | |
| \odot | РСВ | 4 | PYRUS CALLERYANA 'BRADFORD' | BRADFORD FLOWERING PEAR | B & B | 70MM CAL | | | BR. HT. 150CM |
| \odot | QM | 32 | QUERCUS MACROCARPA | BURR OAK | B & B | 70MM CAL | | | NATIVE |
| EVERGREE | EN TREE | s | | | | , | | | |
| | PG | 39 | PICEA GLAUCA | WHITE SPRUCE | B & B | 2M HT | | | NATIVE |
| 0 | PO | 14 | PICEA OMORIKA | SERBIAN SPRUCE | B & B | 2M HT | | | |
| | PP | 11 | PICEA PUNGENS | COLORADO SPRUCE | B & B | 2M HT | | | |
| 3.4 | PS | 19 | PINUS STROBUS | WHITE PINE | B & B | 2M HT | | | NATIVE |
| | CODE | QTY | BOTANICAL NAME | COMMON NAME | CONT. | HT. | SPR. | | REMARKS |
| DECIDUOU (:) | ocm | BS B | AMELANCHIER CANADENSIS | SHADBLOW SERVICEBERRY MULTITRUNK | CONT | 200 CM | Т | T | MULTI-STEM NATIVE |
| \sim | | | CORNUS RACEMOSA | GRAY DOGWOOD | CONT. | 60 CM | | - | NATIVE |
| $ \bigcirc$ | cra | 82 | | | CONT. | 60 CM | - | - | NATIVE |
| (.) | cos | 128 | CORNUS STOLONIFERA | RED OSIER DOGWOOD | CON I. | 60 CM | | 1 | NATIVE |
| GRASSES | | | | | T | | T | _ | T |
| | | | ARIUS 'GLAUCUS' | BLUE LYME GRASS | 2 GAL. | | | 1 | |
| EVERGREE | EN | | | 1 | | | | | |
| • • • | | | RIZONTALIS 'BLUE CHIP' | BLUE CHIP JUNIPER | CONT. | | 60 CM | | |
| SYMBOL | | QTY | BOTANICAL NAME | COMMON NAME | CONT. | нт. | SPR. | SPACING | REMARKS |
| SHRUB AR | EAS | | RIZONTALIS 'LIMEGLOW' | LIMEGLOW CREEPING JUNIPER | 1 GAL. | | | 600 mm | |
| | | | MEGNINEIS EMEGEON | | | | | | |
| GRASS AR | EA | | IS X ACUTIFLORA 'KARL FOERSTER' | KARL FOERSTER FEATHER REED GRASS | 1 GAL. | | T | 500 mm | |
| | - | | CESPITOSA | TUFTED HAIR GRASS | 1 GAL. | | | 600 mm | |
| | | | oco mosn | | | | | | |
| PERENNIA | | | x 'STELLA DE ORO' | STELLA DE ORO DAYLILY | 1 GAL. | | 1 | 300 mm | |
| SYMBOL | CODE | | BOTANICAL NAME | COMMON NAME | CONT. | | | SPACING | REMARKS |
| BULBS | | | | | | | , | , | , |
| | St. | | 'BARRETT BROWNING' | BARRETT BROWNING DAFFODIL | BULBS | | | 200 mm | |
| | 1.5 | | 'DUTCH MASTER' | DUTCH MASTER DAFFODIL | BULBS | | | 200 mm | |
| | | | FORTISSIMO' | FORTISSIMO DAFFODIL | BULBS | | | 200 mm | |
| | | | T A 'DREAM' | DAYDREAM TULIP | BULBS | | | 200 mm | |
| | - 3 | 2 / 1 | ORD' | OXFORD TULIP | BULBS | | | 200 mm | |
| | | | OHAMA' | YOKOHAMA EARLY TULIP | BULBS | | | 200 mm | |

1 SITE PLANTING SCHEDULE

Landscape Architect



9501-9511

TORBRAM ROAD 9501-9511 TORBRAM ROAD BRAMPTON, ONTARIO

Project Number 25-102

Client

DREAM 30 ADELAIDE STREET E. SUITE 301 TORONTO, ON M5C 3H1

C on sultant

HUSSON CIVIL ENGINEERING
200 CACHET WOULD COURT, SUITE 204
MARKHAMA, ON LEG 0.62
TELL: 190.795.2025

Revisions

Stamp



Keymap



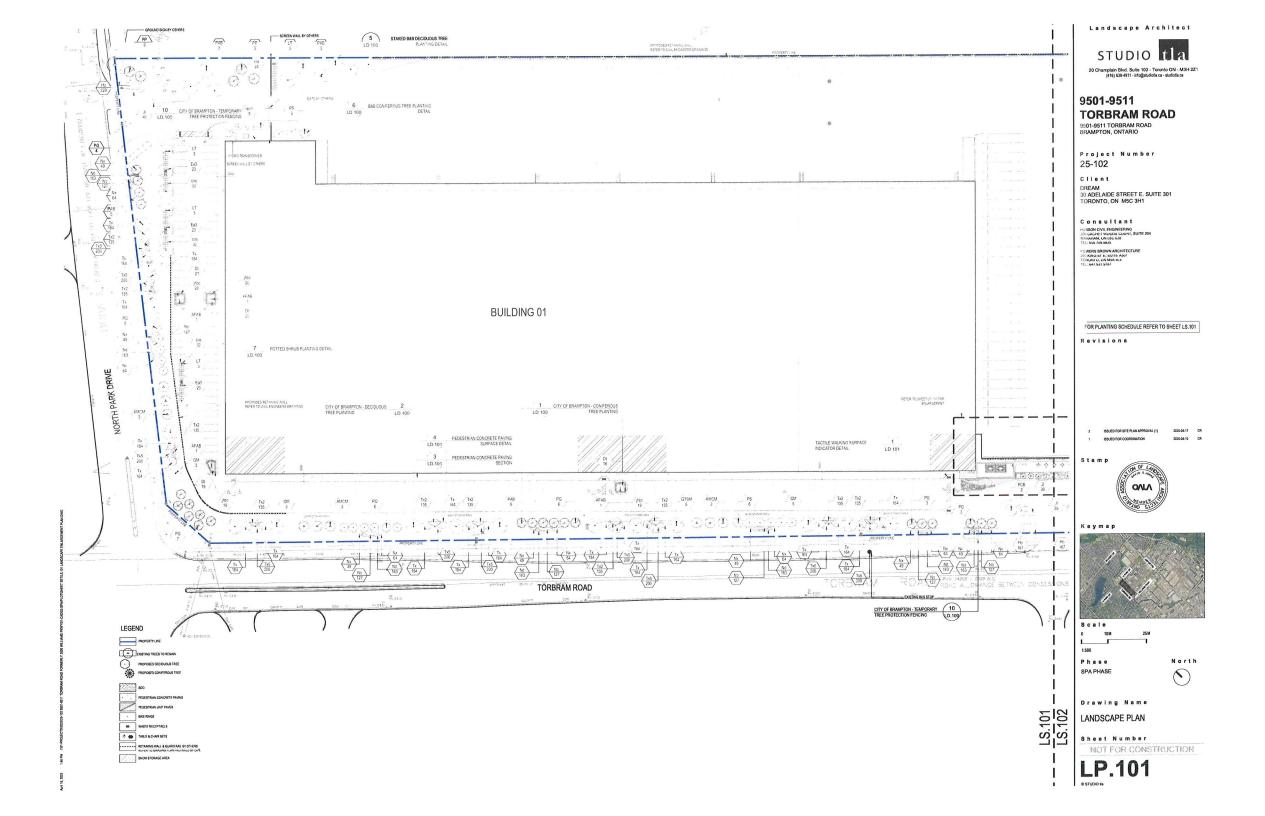
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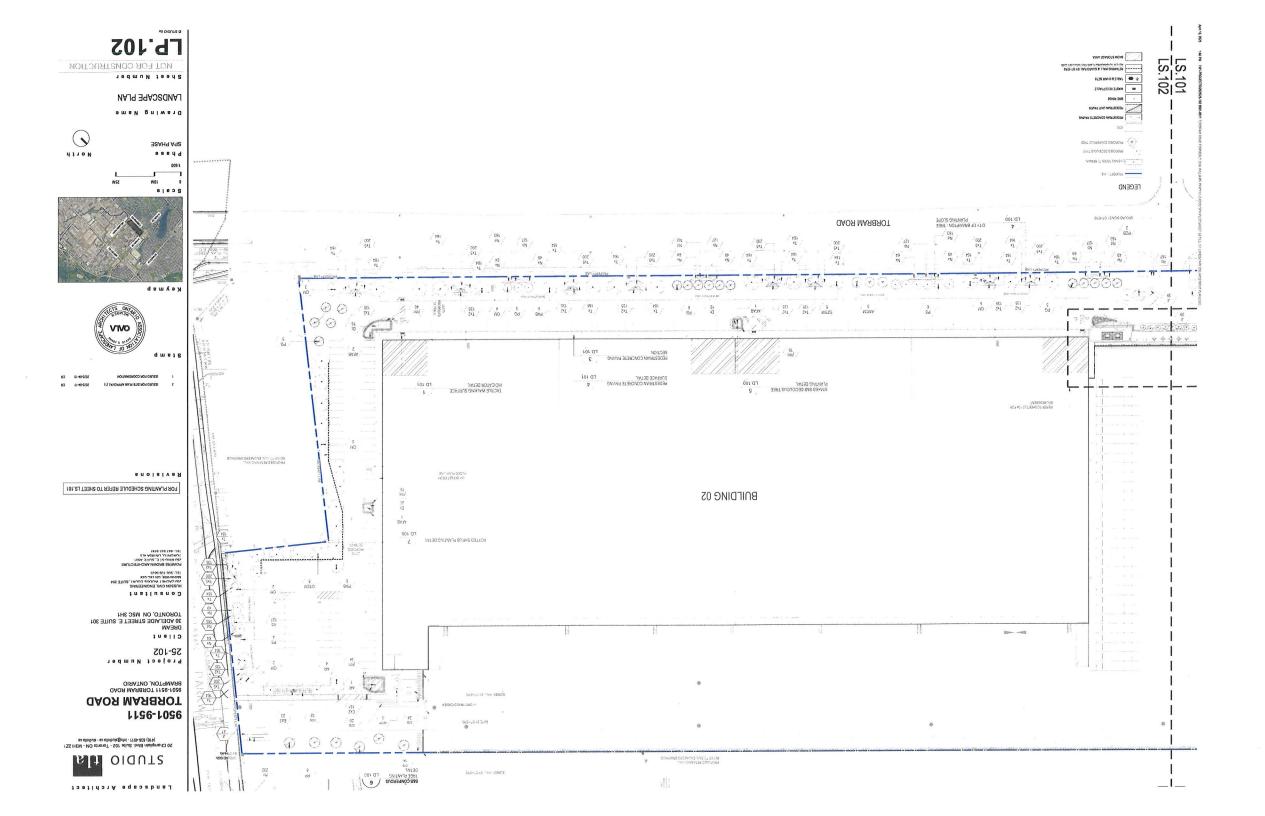
NOT TO SCALE Phase SPA PHASE

Drawing Name

PLANTING SCHEDULE

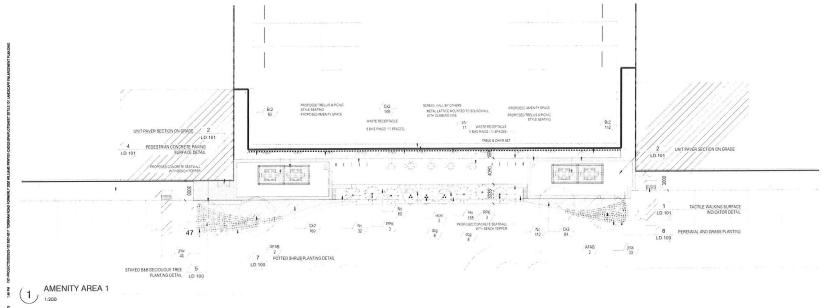
Sheet Number NOT FOR CONSTRUCTION





| SYMBOL | CODE | QTY | BOTANICAL NAME | COMMON NAME | CONT. | CAL. | HT. | | REMARKS |
|----------|---------|-----|--|----------------------------------|-------|----------|-------|---|------------------|
| DECIDUO | US TREE | s | | | | | , | | |
| | AFAB | 4 | ACER FREEMANII 'AUTUMN BLAZE' | AUTUMN BLAZE MAPLE | B & B | 70MM CAL | | | |
| EVERGRE | EN TREE | s | | | | | | | |
| * | PPK | 6 | PICEA PUNGENS 'KOSTERI' | KOSTER'S SPRUCE | B & B | 2M HT | | | |
| SYMBOL | CODE | QTY | BOTANICAL NAME | COMMON NAME | CONT. | HT. | SPR. | | REMARKS |
| NATIVE S | | | | | | | | | |
| () | -acm | 3 | AMELANCHIER CANADENSIS | SHADBLOW SERVICEBERRY MULTITRUNK | CONT. | 200 CM | | | MULTI-STEM NATIO |
| GRASSES | 3 | | | | | | , | | |
| (.) | dcg | 16 | DESCHAMPSIA CESPITOSA 'GOLDTAU' | GOLD DEW TUFTED HAIR GRASS | 2 GAL | | | | |
| 1 | | | - A | | | | | | |
| EVERGRE | =EN | | | | | | | 1 | |
| EVERGRE | jhbc | 79 | JUNIPERUS HORIZONTALIS 'BLUE CHIP' | BLUE CHIP JUNIPER | CONT. | | 60 CM | | |
| | jhbc | 79 | JUNIPERUS HORIZONTALIS 'BLUE CHIP' | BLUE CHIP JUNIPER | CONT. | | 60 CM | | |
| ∰ | jhbc | 79 | JUNIPERUS HORIZONTALIS 'BLUE CHIP' PARTHENOCISSUS TRICUSPIDATA 'VEITCHII' | BLUE CHIP JUNIPER BOSTON IVY | CONT. | | 60 CM | | |

PERFUNIAL



20 Champlain Blvd. Suite 102 - Toronto ON - M3H 2Z1 (416) 638-4911 - info@studiotla.ca - studiotla.ca 9501-9511 **TORBRAM ROAD** 9501-9511 TORBRAM ROAD BRAMPTON, ONTARIO Project Number

Landscape Architect

STUDIO ta

25-102

Client

DREAM 30 ADELAIDE STREET E. SUITE 301 TORONTO, ON M5C 3H1

Consultant

HUSSON CIVIL ENGINEERING 200 CACHET WOODS COURT, SUITE 204 MARKHAM, ON 150 028 TEL: 905.709.5825

Revisions



Keymap

LEGEND

PEDESTRUM CONCRETE PA
PEDESTRUM UNIT PAVER

BRE RINGS

WASTE RECEPTACLE

TABLE & CHAR SETS

Stamp



Scale 01M 5M 10M

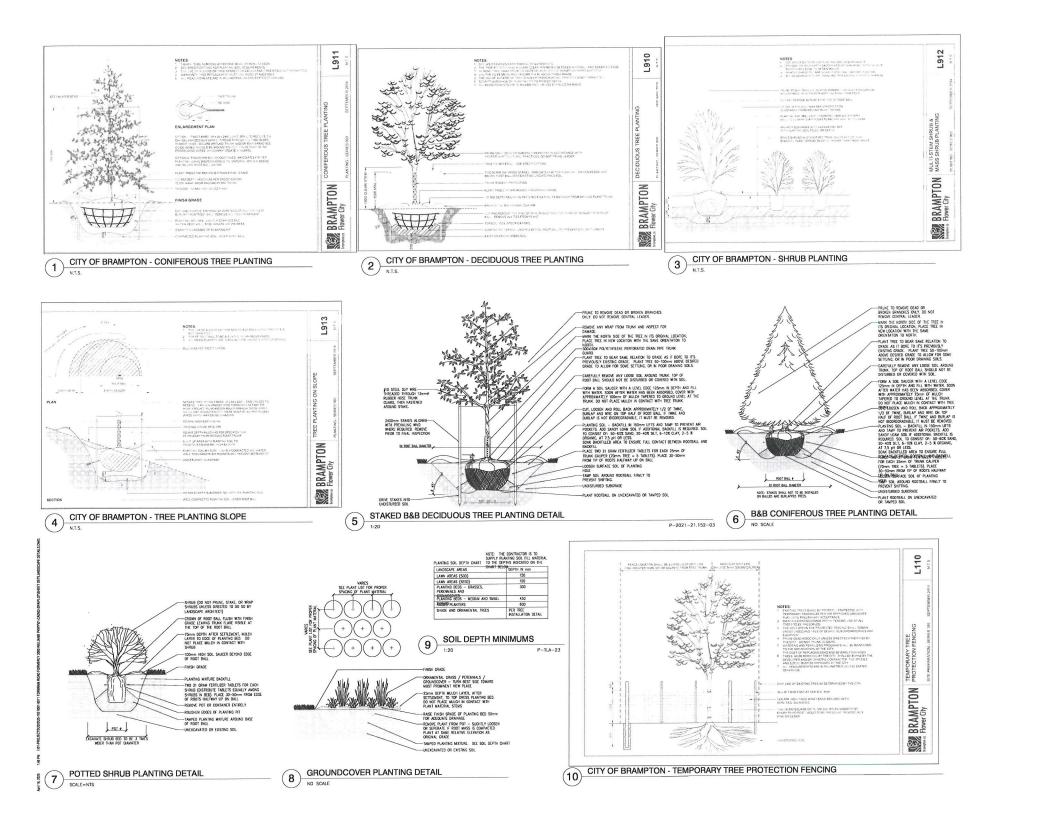
Phase SPA PHASE

North 0

Drawing Name

LANDSCAPE ENLARGEMENT PLAN

Sheet Number NOT FOR CONSTRUCTION



Landscape Architect

STUDIO ta

20 Champlain Blvd. Suite 102 - Toronto ON - M3H 2Z1 (416) 638-4911 - info@studiotla.ca - studiotla.ca

9501-9511 **TORBRAM ROAD**

9501-9511 TORBRAM ROAD BRAMPTON, ONTARIO

Project Number 25-102

Client

DREAM 30 ADELAIDE STREET E. SUITE 301 TORONTO, ON M5C 3H1

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Revisions

ISSUED FOR SITE PLAN APPROVAL (1)

Stamp



Keymap



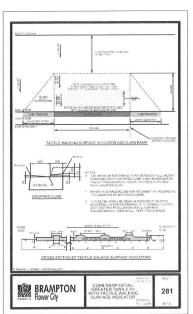
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Phase SPA PHASE North

Drawing Name

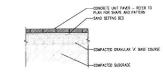
LANDSCAPE DETAILS Sheet Number

NOT FOR CONSTRUCTION

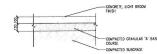


TACTILE WALKING SURFACE INDICATOR DETAIL

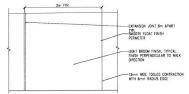
NOTE: UNIT PAYER: UNLOCK HOLLANDSTONE OR APPROVED EQUAL, AS PER LANDSCAPE SPECIFICATIONS



UNIT PAVER SECTION ON GRADE 2 UNIT PA



PEDESTRIAN CONCRETE PAVING SECTION SCALE=1:20



PEDESTRIAN CONCRETE PAVING SURFACE DETAIL 4)

Landscape Architect



9501-9511 **TORBRAM ROAD**

9501-9511 TORBRAM ROAD BRAMPTON, ONTARIO

Project Number 25-102

Client

DREAM 30 ADELAIDE STREET E. SUITE 301 TORONTO, ON M5C 3H1

Consultant

HUSSON CIVIL ENGINEERING 200 CACHE! WOODS COURT, SUITE 204 MARKHAM, ON LEC 028 TEL: 906-709-5825

POWERS BROWN ARCHITECTURE 260 KING 51 E, SUITE A501 TORONTO, ON MSA 4L5 TEL: 647.931.9787

Revisions



Keymap

Stamp



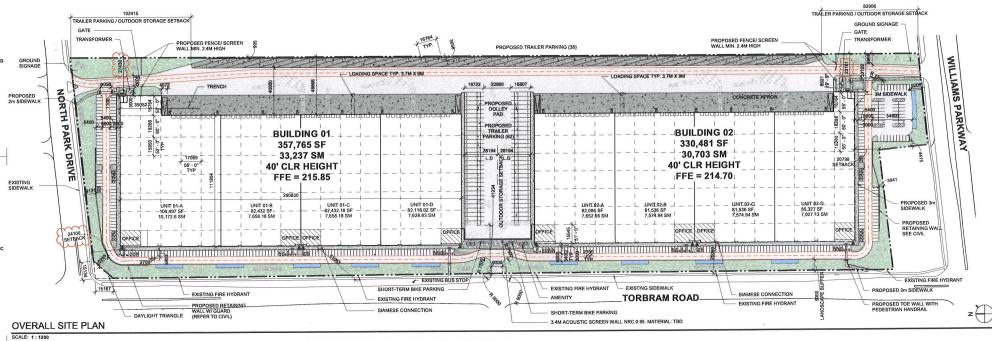
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Phase SPA PHASE

North

Drawing Name LANDSCAPE DETAILS

Sheet Number NOT FOR CONSTRUCTION



| GROSS FLOOR AI | REAS: |
|-----------------------|--------------------------------|
| BUILDING 01 : | 33,237 m² (357,765 SF) |
| BUILDING 02: | 30,702 m² (330,481 SF) |
| TOTAL: | 63,939 m² (688, 246 SF) |
| SITE COVERAGE: | 49% |
| BUILDING HEIGHT | 14.31 m (REQUIRED MAX. 10.8 m) |

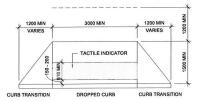
| BUILDING SETBACKS : | | |
|---------------------|---------------|----------|
| | PROPOSED | REQUIRED |
| WILLIAMS PARKWAY | 20.8 m | 30 m |
| TORBRAM ROAD | 24.2 m | 9 m |
| NORTH PARK DRIVE | 24.1 m | 25 m |
| PROPOSED DISTANCE | FOR OUTDOOR S | TORAGE: |
| | PROPOSED | REQUIRED |
| WILLIAMS PARKWAY | 82.9 m | 150 m |
| TORBRAM ROAD | 41.4 m | 90 m |
| NORTH PARK DRIVE | 102.4 m | 25 m |
| AIRPORT ROAD | N.A. | 180 m |

| BUILDING 01: | | 246 /PE "A" BARRIER /PE "B" BARRIER D12 EV) |
|------------------|-----|--|
| BUILDING 02: | | 231 PE "A" BARRIER PE "B" BARRIER D12 EV) |
| TOTAL PARKING: | 381 | 477 |
| TRAILER PARKING: | 100 | |
| LOADING DOCKS: | 102 | |

| AS PER BRAMPTON ZONING BY-LAWS, SECTION |
|---|
| 30.0 GENERAL PROVISIONS FOR INDUSTRIAL |
| ZONES - ITEM 30.5 (PARKING SPACES - |
| WAREHOUSE): |
| OVER 20,000 SQUARE METRES: 168 PARKING |
| SPACES PLUS 1 PARKING SPACE PER 170 |
| SQUARE METRES GROSS FLOOR AREA OR |
| PORTION THEREOF THAT IS OVER 20,000 |
| SQUARE METRES |
| BUILDING 01: |
| 168 SPACES (FOR FIRST 20,000 m 2) + ((13,237/170) |
| = 78 SPACES) = 168+78 = 246 SPACES |
| BUILDING 02: |
| 168 SPACES (FOR FIRST 20,000 m 2 + ((10,703/170) |
| = 63 SPACES) = 168+63 = 231 SPACES |
| |

| AREA OF EACH | TENANT SPACE: | | | |
|---------------|---------------------------------------|------------|---|---|
| BUILDING 01 : | | | V-1V2-1C10-1-1-1-1-1-1-1-1-1-1-1-1-1-1-1-1-1- | |
| UNIT NUMBER | AREA | DOCK DOORS | AREA/ DOCK DOORS | OFFICE AREA |
| UNIT 01 -A | 109,497 SF (10,172.6 m ²) | 09 | 1/12,216 SF (1/1,130 m 2) | 3,441 SF or 319.67 m ² (3.14%) |
| UNIT 01 - B | 82,432 SF (7,658.18 m ²) | 14 | 1/5,888 SF (1/547 m ²) | 2,604 SF or 241.91 m ² (3.15%) |
| UNIT 01- C | 82,432 SF (7,658.18 m ²) | 15 | 1/5,495 SF (1/510 m ²) | 2,604 SF or 241.91 m ² (3.15%) |
| UNIT 01 -D | 82,984 SF (7,709.46 m ²) | 13 | 1/6,383 SF (1/593 m ²) | 2,673 SF or 248.32 m ² (3.18%) |
| TOTAL: | 357,345 SF (33,198 m²) | 51 | 1/6,742 SF (1/603 m ²) | 11,322 SF or 1,051 m 2 (3.16%) |

| AREA OF EACH TENANT SPACE: BUILDING 02: | | | | | |
|---|---------------------------------------|----|------------------------------------|---|--|
| | | | | | |
| UNIT 02 -A | 82,082 SF (7,652.66 m ²) | 13 | 1/8,314 SF (1/586 m ²) | 2,587 SF or 240.34 m ² (3.15%) | |
| UNIT 02 - B | 81,536 SF (7,574.94 m ²) | 15 | 1/5,435 SF (1/504 m ²) | 2,520 SF or 234.11 m ² (3.09%) | |
| UNIT 02- C | 81,536 SF (7,574.94 m ²) | 13 | 1/6,272 SF (1/582 m ²) | 2,520 SF or 234.11 m ² (3.09%) | |
| UNIT 02-D | 85,327 SF (7,927.13 m ²) | 10 | 1/8,232 SF (1/764 m ²) | 2,587 SF or 240.34 m ² (3.03%) | |
| TOTAL: | 330,481 SF (30,702.6 m ²) | 51 | 1/6,480 SF (1/602 m ²) | 10,214 SF or 948.9 m ² (3.09%) | |



C4 CURB CUT SCALE: 1:50

Suite 206 Toronto, Ontario M5A 3S5 647.931.9787 brown archit www.powersbrown.com ecture PROJECT TITLE DREAM 9501-9511 TORBRAM ROAD, BRAMPTON A PROJECT FOR DREAM GENERAL NOTES DESIGNATED FIRE ACCESS LANE - MINIMUM 6M PROPOSED SNOW STORAGE ELECTRIC VEHICLE EV CARPOOL FIRE HYDRANT SIAMESE CONNECTION ▲ PRINCIPLE ENTRANCE A PROVIDE FIRE LANE STRIPING PER LOCAL JURISDICTION B FIRE DEPARTMENT ACCESS LANE SITE CONCRETE PAVING TO BE CAPABLE OF WITHSTANDING 90,000 LBS UNDER ALL WEATHER CONDITIONS C FIRE LANE CENTRE LINE RADII TO BE 12 M 2700 TYP 7400 TYP 400 1400 PE A 1000 TYPE B SHEET NAME OVERALL SITE PLAN SHEET NUMBER AS101

powers

411 Richmond Street E

Zoning Non-compliance Checklist

Applicant: Williams Parkway Torbram Holdings LP

Address: 9501-9511 Torbram Rd

Zoning: M2-305 / M2-307 / Floodplain

By-law 270-2004, as amended

File No. A-2025-0050

Received / Revised

JUN 0 4 2025

Committeee of Adjustment

| Category | Proposal | By-law Requirement | Section # |
|--|--|--|------------------|
| FLOODPLAIN SETBACK | To permit a 0.0m setback to a Floodplain zone | Whereas the By-law required a minimum setback of 30m | 305.2(B) |
| BUILDING SETBACKS FRONT / SIDE / REAR | To permit a street line setback of 24.1m from North Park Drive | Whereas the By-law requires a minimum street line setback of 25m from North Park Drive | 305.2(a)1 |
| BUILDING HEIGHT | To permit a building height of 14.31m | Whereas the By-law permits a maximum building height of 9.0m | 30.15 |
| TRANSFORMER SETBACKS | To allow side yard setbacks of 27.43m and 23.18m to the proposed hydro transformers | Whereas the By-law requires a minimum side yard setback of 30.0m | 6.10 305.2(B) |
| LANDSCAPED OPEN SPACE | To provide 3.5m of landscaping along Williams Parkway except at approved access locations | Whereas the By-law requires a minimum 30m wide landscaped buffer along Williams Parkway | 305.2(c)1 |
| LANDSCAPED OPEN SPACE | To provide a landscape buffer having a minimum width of 9.0m along Torbram Road except at approved access locations with no berm | Whereas the By-law requires a minimum width of 75.0m along Torbram Road as a continuous, uninterrupted bermed strip | 305.2(c)2 |
| LANDSCAPED OPEN SPACE | To provide 9.0m of landscaping along North Park Drive except at approved access locations with no minimum distance | Whereas the By-law requires a minimum width of 60.0m along the North Park Drive for a minimum distance of not less than 150.0m, and not more than 240.0m east of Torbram Road, and 15.0m for the remaining distance; | 305.2(c)3 |
| OUTSIDE STORAGE | To permit outside storage 20m from Williams Parkway and 41m from Torbram Road with no berm | Whereas the By-law requires outside storage to be setback a minimum of 150m from Williams Parkway and 90m from Torbram Road with a berm not less than 2.4m in height | 305.2(d) |
| PARKING | To provide 381 parking spaces | Whereas the By-law requires 457 parking spaces | 30.5 |
| FLOODPLAIN | To permit a building within a Floodplain zone | Whereas the By-law prohibits buildings within a Floodplain zone | 44.2 |

| Todd Payne | |
|--------------------|---|
| Reviewed by Zoning | |
| June 3, 2025 | _ |
| Date | |

Zoning Non-compliance Checklist

File No. A-7075-0050

Applicant: Williams Parkway Torbram Holdings LP

Address: 9501-9511 Torbram Rd

Zoning: M2-305 / M2-307 / Floodplain

By-law 270-2004, as amended

| Category | Proposal | By-law Requirement | Section # |
|--|--|--|------------------|
| FLOODPLAIN SETBACK | To permit a 0.0m setback to a Floodplain zone | Whereas the By-law required a minimum setback of 30m | 305.2(B) |
| BUILDING SETBACKS FRONT / SIDE / REAR | To permit a street line setback of 24.1m from North Park Drive | Whereas the By-law requires a minimum street line setback of 25m from North Park Drive | 305.2(a)1 |
| BUILDING HEIGHT | To permit a building height of 14.31m | Whereas the By-law permits a maximum building height of 10.8m | 305.2(e) |
| TRANSFORMER SETBACKS | To allow side yard setbacks of 27.43m and 23.18m to the proposed hydro transformers | Whereas the By-law requires a minimum side yard setback of 30.0m | 6.10 305.2(B) |
| LANDSCAPED OPEN SPACE | To provide 3.5m of landscaping along Williams Parkway except at approved access locations | Whereas the By-law requires a minimum 30m wide landscaped buffer along Williams Parkway | 305.2(c)1 |
| LANDSCAPED OPEN SPACE | To provide a landscape buffer having a minimum width of 9.0m along Torbram Road except at approved access locations with no berm | Whereas the By-law requires a minimum width of 75.0m along Torbram Road as a continuous, uninterrupted bermed strip | 305.2(c)2 |
| LANDSCAPED OPEN SPACE | To provide 9.0m of landscaping along North Park Drive except at approved access locations with no minimum distance | Whereas the By-law requires a minimum width of 60.0m along the North Park Drive for a minimum distance of not less than 150.0m, and not more than 240.0m east of Torbram Road, and 15.0m for the remaining distance; | 305.2(c)3 |
| OUTSIDE STORAGE | To permit outside storage 20m from Williams Parkway and 41m from Torbram Road with no berm | Whereas the By-law requires outside storage to be setback a minimum of 150m from Williams Parkway and 90m from Torbram Road with a berm not less than 2.4m in height | 305.2(d) |
| PARKING | To provide 381 parking spaces | Whereas the By-law requires 457 parking spaces | 30.5 |
| FLOODPLAIN | To permit a building within a Floodplain zone | Whereas the By-law prohibits buildings within a Floodplain zone | 44.2 |

Reviewed by Zoning

May 20, 2025

Date