



Heritage Impact Assessment, 8000 Dixie Road, Part of Block A, Plan 676, Formerly Part Lot 1 (East), Concession 3, Geographic Township of Chinguacousy, Now City of Brampton, Regional Municipality of Peel

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Proponents: Ryan Smele, Panattoni Development Company

Address: 185 The West Mall, Suite 860, Toronto ON M9C 5L5

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- Appendix A - Qualifications
- Appendix B – Development Plan
- Appendix C - Brampton Heritage Inventory Listing Candidate Summary document

1. Executive Summary

Parslow Heritage Consultancy, Inc. (PHC) was retained by Panattoni Development Company (the Proponent) to undertake a Heritage Impact Assessment (HIA) for the property located at 8000 Dixie Road, Brampton, Ontario (Subject Property). The Proponent is proposing the redevelopment of 8000 Dixie Road following the upcoming closure of the Ford Motor Company parts distribution centre in March 2023. The proponent is undertaking this study as a condition of development in order to obtain a demolition permit and subsequent building permit for the site.

The purpose of this assessment is to review relevant historical documents and identify any cultural heritage resources associated with the property. To evaluate the cultural heritage value or interest (CHVI) associated with the property, provisions in the *Ontario Heritage Act* (OHA) under Regulation 9/06 and the *Planning Act* (1990) were applied.

The Subject Property is located on the northwest corner of the intersection of Steeles Avenue East and Dixie Road in Brampton, Ontario, south of the GO Transit Kitchener rail corridor. The property contains a mid-twentieth century administration building constructed in the International Style, and an associated warehouse. The property was purpose built by the Ford Motor Company in 1963 and Ford has maintained ownership of it until present. The surrounding area consists of light manufacturing and industrial parks east of Highway 410. The Subject Property was added to the City of Brampton's Municipal Heritage Resources – 'Listed' Heritage Properties in 2005 (Brampton Heritage Board, 2021).

Given the contextual value of at 8000 Dixie Road, it is recommended that the proposed re-development be approved and that a commemoration be undertaken as part of the redevelopment.

The following mitigation options in ranked order based on heritage industry standards are considered and explored in detail in Section 8:

1. Retain the property in its current configuration and restore it to its 1963 appearance. The interior of the administration building could be retrofit and rented out as office space and the existing warehouse could be updated and partitioned internally for use by multiple clients.
2. Retain the administration building and demolish the warehouse. The interior of the administration building would then be renovated, and the exterior restored to its 1963 configuration. Re-development could then take place surrounding the administration building while retaining the current setback and visibility of the administration building from Steeles Avenue East and Dixie Road.
3. Demolish the extant buildings and facilitate development as proposed. The re-development could conserve the contextual value of the property by establishing a community art installation at the corner of Steeles Avenue East and Dixie Road. This installation would include an interpretive display outlining the history of the development of Bramalea and the role Ford Motor Company of Canada played in the success of Bramalea.

The report makes the following recommendations:

1. The feasibility of integration of the extant structure into the redevelopment be assessed.
2. Should integration not be feasible it is recommended that 8000 Dixie Road be subject to salvage during the demolition process. To facilitate this a Salvage and Documentation Plan should be created as the current structure contains many components that could be diverted from landfill and recycled. The Salvage and Documentation Plan can be scoped as additional documentation is not required as the property and existing structures are fully documented as part of this report. Salvageable materials including but not limited to:
 - ▶ Steel framing components
 - ▶ Exterior metal sheeting
 - ▶ Electrical and plumbing components
 - ▶ HVAC system
 - ▶ Industrial engines and generators

If possible salvaged materials should be incorporated into the public display commemorating the contextual value of the property to the development of Bramalea.

3. A demolition permit be issued for 8000 Dixie Road, with a condition of material salvage.
4. A commemoration plan be undertaken, and a commemoration be erected as part of the redevelopment. Commemoration needs to address the origins of any salvaged materials incorporated into the installation, recognition of the importance of Ford Motor Company Canada to the local community and provide a history of the development of Bramalea, Canada's first planned satellite community. It is recommended that commemoration of 8000 Dixie Road be located within sight of the intersection of Dixie Road and Steeles Avenue East. Potential commemoration options include but are not limited to:
 - i. A landscape feature with interpretive signage that is accessible to the public.
 - ii. A sculpture or art installation with descriptive plaque
5. The City of Brampton's municipal register of Cultural Heritage Resources: 'Listed' Heritage Properties be updated, and 8000 Dixie Road be removed.
6. As 8000 Dixie Road is currently occupied the production of a Conservation Plan is not recommended.

2. Personnel

| | |
|---|-------------------------------------|
| Carla Parslow, Ph.D., CAHP | Senior Cultural Resource Specialist |
| Christopher Lemon, B.Sc., Dip. Heritage, CAHP | Lead Cultural Heritage Specialist |
| Renee Hendricks, M.A. | Researcher and Archivist |
| Adam Long M.Sc. | Report Review |

Acknowledgements

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|-------------------|---|
| Ryan Smele | Panattoni Development Company |
| Shelby Swinfield | Assistant Heritage Planner, City of Brampton |
| Merissa Lompart | Assistant Heritage Planner, City of Brampton |
| Nick Moreau | Archivist, Peel Art Gallery, Museum, and Archives |
| Courtney McAlpine | Senior Digital Archivist, Ford Motor Co. Archives |

3. Background

Parslow Heritage Consultancy, Inc. (PHC) was retained by Panattoni Development Company (Proponent) to undertake a Heritage Impact Assessment (HIA) for the property located at 8000 Dixie Road, Brampton, Ontario (Subject Property). The Proponent is proposing the redevelopment of 8000 Dixie Road following the planned closure of the Ford Motor Company parts distribution centre in March 2023. The proponent is undertaking this study as a condition of development in order to obtain a demolition permit and subsequent building permit for the site.

The Subject Property is currently a listed property as identified on the City of Brampton's *Municipal Register of Cultural Heritage Resources 'Listed' Heritage Properties* (COB,2021). Listing data is available in Appendix C.

The purpose of this assessment is to review relevant historical documents and identify any cultural heritage resources associated with the property. To evaluate the cultural heritage value or interest (CHVI) associated with the property, provisions in the *Ontario Heritage Act* (OHA) under Regulation 9/06 and the *Planning Act* (1990) were applied.

The site visit to assess the CHVI associated with 8000 Dixie Road was conducted by Chris Lemon on 24 November 2021.

Documentation of the property took the form of high-resolution photographs using a Nikon D5600 DSLR camera and the collection of field notes.

The assessment strategy was derived from the *National Historic Parks and Sites Branch Canadian Inventory of Historic Buildings* (Parks Canada, 1980), *Well Preserved: The Ontario Heritage Foundation Manual on the Principles and Practice of Architectural Conservation* (Fram, 2003), the *Historic American Building Survey – Guide to Field Documentation* (HABS, 2011), and the *Standards and Guidelines for the Conservation of Historic Places in Canada* (Parks Canada, 2010). All accessible areas of the property and associated structures were accessed and documented.

3.1 Proponent Contact Information

Ryan Smele (Development Manager)
Panattoni Development Company
185 The West Mall, Suite 860
Toronto ON, M9C 5L5
Tel: 416-915-1986
Email: rsmele@panattoni.com

4. Introduction to the Subject Property

The Subject Property at 8000 Dixie Road comprises approximately 60 acres at the northwest corner of the intersection of Steeles Avenue East and Dixie Road, in the City of Brampton. The property is located south of the GO Transit Kitchener rail corridor.

The Subject Property is currently listed on the City of Brampton's *Municipal Heritage Register of Cultural Heritage Resources 'Listed' Heritage Properties* (COB, 2021). No additional information is provided in the heritage listing. However, Merissa Lompart, Assistant Heritage Planner at the City of Brampton, was able to provide a Brampton Heritage Inventory Listing Candidate Summary document from 2005 that briefly relates the history of the extant structure on the Subject Property. The City of Brampton file identifies 8000 Dixie Road as a Class B heritage structure that is worthy of preservation (Appendix C).

8000 Dixie Road was previously subject to CHIA in 2017 as part of a development proposal that was abandoned (ASI, 2017).

The Subject Property was acquired for industrial development in the mid-twentieth century as part of the development of Bramalea. The warehouse and administration building that currently occupies the property was constructed by the Ford Motor Company, commencing operation in 1963. The property currently contains an 865,439 square foot warehouse connected to a 58,832 square foot administration building.

The Subject Property is visible from both Steeles Avenue East and Dixie Road. The administration building is set well back from the street with an expanse of manicured lawn separating the structure from the road right of way.

As of the date of this assessment (December 2021) the Subject Property continues to be occupied and to serve as a parts distribution centre for Ford Motor Company of Canada.

4.1 The Property

The Subject Property is located on the northwest corner of the intersection of Dixie Road and Steeles Avenue East, south of the GO Transit Kitchener rail line. The property contains a mid-twentieth century warehouse and administration building. Both the warehouse and administration building reflect the architectural style known as the International Style (Blumenson 1990); the administration building is the better example of the style. The surrounding area is comprised of light manufacturing, warehouses, and industrial parks.

The property is currently occupied and continues to operate as a regional distribution centre for Ford Motor Company of Canada.

A 1968 aerial image shows the warehouse occupying a large portion of the current structure's footprint, although what was the original back parking lot has been covered by a warehouse addition to the back of the property as seen in current satellite views available through Google Earth.

The structure is in average condition for its age; it has been subject to regular maintenance but has not been updated or subject to significant retrofit since construction.

4.2 Site History

The Subject Property at 8000 Dixie Road is located in part of Block A, Plan 676, formerly the east half of Lot 1, Concession 3 (east of Hurontario Street), in the Geographic Township of Chinguacousy, now the Regional Municipality of Peel. Early historical land use was agricultural, although the lot is now occupied by several industrial facilities and warehouses, including the Ford Motor Company's Sales and Parts Distribution Centre of Bramalea, Brampton's first planned satellite community.

4.2.1 Nineteenth Century History of the Property

Lot 1 in the 3rd Concession sat on the boundary line between Chinguacousy and Toronto Townships in a former rural settlement known as Fraser's Corners. Named for its first settler, Robert Fraser, the settlement had a church by 1854 although no trace of it remains today. The 1873 County of Peel Directory describes Fraser's Corners as a "small village on the Town line between Toronto and Chinguacousy," four miles from Brampton and four miles from Malton with a population of around 80 persons (Lynch, n.d.:101).

The first Euro-Canadian settler on the 200-acre parcel was Robert Trimble, who was granted patent by the Crown in September 1836. An immigrant from County Fermanagh, Ireland, Trimble initially settled on Lot 24, Concession 6 (EHS) in 1819 but had apparently moved to Lot 1, Concession 3 (EHS) by 1821, where he testified before the Home District that "*the Settling Duty is done on that lot, No. 1, in the 3rd concession of Chinguacousy east of the Senter Roade [Hurontario]...a Habitable House built 20 by 28*" (Bull, n.d.:5-6). Robert Trimble served the community as a pathmaster, an official in charge of supervising road construction and repair, and as a Constable for the Home District Quarter Sessions in the last half of the 1830s. There is not much information available regarding Robert Trimble and it is not clear when he died, but the Abstract Index to Deeds in Ontario lists a will in connection with Trimble in 1840, possibly indicating his death.

In 1842 William Trimble purchased the northwest quarter of the property, or 50 acres, from Robert. William Trimble was likely one of Robert's sons and had a reputation as a money lender within the community. A Methodist and Liberal party member, he married Mary Smyth (1819 – 1902) and had three children: son Robert John Trimble, a doctor who practiced in Claude and Queenston, and daughters Mary, and Margaret. Both daughters remained unmarried. William Trimble appears in both the 1851 Agricultural and Personal Census of Upper Canada as owning 100 acres on Lot 1, Concession 3 -60 of which were under cultivation. He is listed as a 40-year-old Irish farmer and a member of the Methodist church. His wife, 30-year-old Mary was a Presbyterian and they had three children: Margaret (5), Robert (3), and Mary (1).

The other inheritor of Robert Trimble Sr.'s property appears to be another son named George Trimble, as he sold 64/100 acre to the Toronto and Guelph Railway in 1853 for the construction of the Kitchener line (and which still exists as part of the GO Transit rail network, located directly to the north of the Subject Property). George was born in 1810 and immigrated from County Fermanagh with his parents as a child. He married Jane Grasby (1814 – 1903) at Erindale in 1831,

although some records list her maiden name as Foster. According to genealogical sources the couple had ten children, although only three that died in childhood are listed on George's headstone. George and Jane's children were Mary Ann (1833 – 1842), Margaret (b. 1835), Robert (1838 – 1915), Mary A. (1840 – 1871), John (1843 – 1899), Francis (1845 – 1851), Jane (1847 – 1857), George Jr. (1849 – 1891), Dorothy (1851 – 1916), and Thomas (b. 1856). George Trimble, also a devout Methodist, was appointed pathmaster in 1839 and moved to Albion Township the same year, where he led classes at Reed's Chapel. At some point George Trimble returned to the baseline between Chinguacousy and Toronto Townships, where he lived until his death in 1855 at age 45. George Tremaine's 1859 Atlas of Peel County shows the eastern quarter of Lot 1 as owned by George Trimble's Estate, while the centre 50 acres was owned by Robert Trimble.

According to genealogical records, Robert Trimble was one of George's sons, and was born in Ontario in 1838. He married Sophia Wright (1841 – 1886) in 1859 in Peel County, and the couple had thirteen children: James (b. 1857), George W. (1860 – 1897), Robert W. (b. 1861), William (b. 1864), Lucy (b. 1864), Margaret (b. 1866), Jane (b. 1869), Eva (b. 1871), Mary Alice (1873 – 1971), Millie (1875 – 1925), Melly (b. 1876), Millie May (b. 1877), and Oliver (b. 1878). However, the Ontario Deed Abstract Index does not list Robert Trimble as a landowner until 1873, when he willed portions of the property to a Mary Anne Trimble, possible a sister or a cousin. The 1861 Canada Census, both Agricultural and Personal, indicate a person named George Grasby lived on Lot 1, Concession 3. Grasby was the maiden name of George's wife Jane, and this individual could be one of her relatives occupying the property as a tenant farmer. It would not be George Trimble, as he had died six years previously. Grasby held 100 acres, 70 of which was under cultivation. According to the census Grasby was a 27-year-old farmer originally from England and married to Jane Grasby, age 38, originally from Ireland. The couple lived with relatives Mary Anne (20), John (19, labourer), George (12), Dorothy (10), Thomas (8), James (6), Charles (4), and Edward (2). Some of these people were likely children of George and Jane Grasby, but the census is unclear on the relationships of the others. The whole family are listed as Methodists, and they live in a one-storey brick house.

George Grasby also appears in the 1871 Canada Census living with his wife Jane and one son, Charles (14), although it is unclear if they are still living on the Subject Property. William Trimble, a 53-year-old Irish Methodist farmer, appears in the census as living with his wife Mary (age 50, also an Irish Methodist) and children Margaret (21), Robert (19, farmer), Mary (16), and Elizabeth (13). William Mann, a 23-year-old Scot, is employed by the William Trimble family as a farm labourer. Robert Trimble, the son of George, is also listed in the census as a farmer, and lived with his wife Sophia (age 31) and children George (11), Robert Jr. (9), William (7), James (14), Margaret (5), Sophia Jane (3), and Eva, an infant.

In 1876 William Trimble sold the 50 acres of the "east half of the east half" to Thomas Tales, and Robert Trimble also sold a small parcel to Tales four years later in 1880. The 1877 Pope Atlas of Peel County indicates Robert Trimble as owning the west quarter of the east half, and Thomas Tales and the owner of the eastern 50 acres. Tales, spelled "Tayles" by William Perkins Bull, was born in 1835 in Lincolnshire, England, and was living in Etobicoke in 1871. He married Jane Graham (b. 1844) in 1866, and the couple had one daughter named Sarah Elizabeth (1867 – 1956). The 1881 Canada Census lists Thomas Tales, age 40, as living on part of Lot 1 with his wife Jane (36) and

daughter Sarah “Lizzie” (age 14). Robert Trimble, then aged 42, was also still living on the property with his wife Sophia (40), and their children George (21, blacksmith), Robert (19, boiler worker), William (17, farmer), Lucy (14, dress maker), Jane Sophia (12), Eva (10), Mary (8), and Millie May (4). The 1891 Census only lists Robert Trimble as habiting a one-storey, five room wooden house with his daughters Eva (21) and Millie (14), as his wife had died by that point. Thomas Tales was also likely still living there but that was unable to corroborated with any other data.

4.2.2 Twentieth Century History of the Property

In 1899 Tales sold his 50-acre holding on Lot 1 to his nephew Thomas G. Baldock. Baldock was born in 1874 in Ontario and was the son of Tales’ wife’s sister Elizabeth Graham and her husband Isaac Baldock. The 1901 Canada Census indicates that Baldock was 27 that year and was working as a farmer. He lived on Lot 1 with his mother Elizabeth (51) and his siblings Herbert (21), Nellie (16), and Lillie (13). Robert Trimble was also still residing on Lot 1 and lived with his unmarried daughters Eva (30) and Mille (24). In 1906 Trimble also sold his 50 acres to Thomas Baldock, meaning that Baldock owned the entire 100 acres of the eastern half of Lot 1. Baldock appears to be a bit of a land speculator, as the 1911 Canada Census records him as living on Lot 3 in the 3rd Concession with his wife Martha (age 35) and renting out the eastern half of Lot 1 to two tenant farmers. Olliver Osborne, age 35, was listed as residing on Lot 1 with his wife Emily (45), married daughter Ethel Everal (23), Ethel’s son Alfred (1), and their other children Emmlean (20) and Frederick (17, labourer). William Sinclair, a 26-year-old Ontarian farmer was also living on the property, likely as a tenant, with his wife Helen/Hellan (age 26) and their children James (3) and Edwin (1). A domestic servant named Florence Bailey (16) and a farmhand named Frederick Black (22) also lived with the Sinclairs. By 1921, however, only Thomas Baldock, his wife Martha, and their children Graham (9) and Martha (6) are listed as living on the east half of Lot 1 in a stand-alone brick dwelling, possibly with six rooms.

In 1923 Martha Baldock sold the eastern 100 acres to Cyril H. Middlebrook. Middlebrook was born in Ontario in 1895 to Thomas Middlebrook and Mary Ann Banane. He married Annie McAllister in Peel County in 1915 and the couple had two children, but neither survived past infancy. In 1927 Middlebrook sold the eastern half to Alfred Cowton (1860 – 1935), the son of William Cowton and Susanna Ezard. Cowton had married Sarah Erwin in 1892 in Peel, and the couple had two children: Stanley Frederick (1903 – 1967) and Frederick Erwin (b. 1905). In 1938, after Cowton’s death, his executor sold the eastern half of Lot 1 to Stella B. Middlebrook, Cyril Middlebrook’s sister. Stella in turn sold to Thomas M. Robinson in 1939, and Robinson sold to Lillian & Clarence Prosser in 1943. The Prossers only owned the subject parcel for two years and sold to John M. Moddison in 1945. Moddison sold all 100.691 acres to Bayton Holdings in 1957, who sold to Bramalea Consolidated Developments in 1958, who in turn sold to Ford Motors in 1963 when the current structure in the Subject Property was constructed. (A complete list of land transactions pulled from the Ontario Deed Abstract Index is contained in Section 4.2.4 (Ownership History) below.

Bayton Holdings was the pre-cursor to Bramalea Consolidated Developments, and was the brainchild of Thomas Sprigg, George Clarke, and Dr. James Sihler. In the 1950s the three men envisioned the first planned satellite community from Brampton and began buying up farmland as real estate agents. The company, Bayton Holdings, was named for the intersection of Bay and Wellington Streets in Toronto. Bramalea Consolidated Developments was founded in 1957 for the

purpose of building a self-contained satellite city outside of Toronto and bought most of its real estate in the Bramalea area from Bayton Holdings. Bramalea Consolidated was first listed as a publicly traded company in the 1960s and played a huge role in enticing companies in moving to Bramalea, of which Northern Electric and Ford Motor Company were the first to do so. The company continues to invest in numerous business ventures and is active in real estate both in Canada and internationally.

After Ford Motors purchased the eastern half of Lot 1, Concession 3 in 1963, it began construction on a 60-acre facility completed in July 1964. This is the current facility still in operation on the Subject Property.

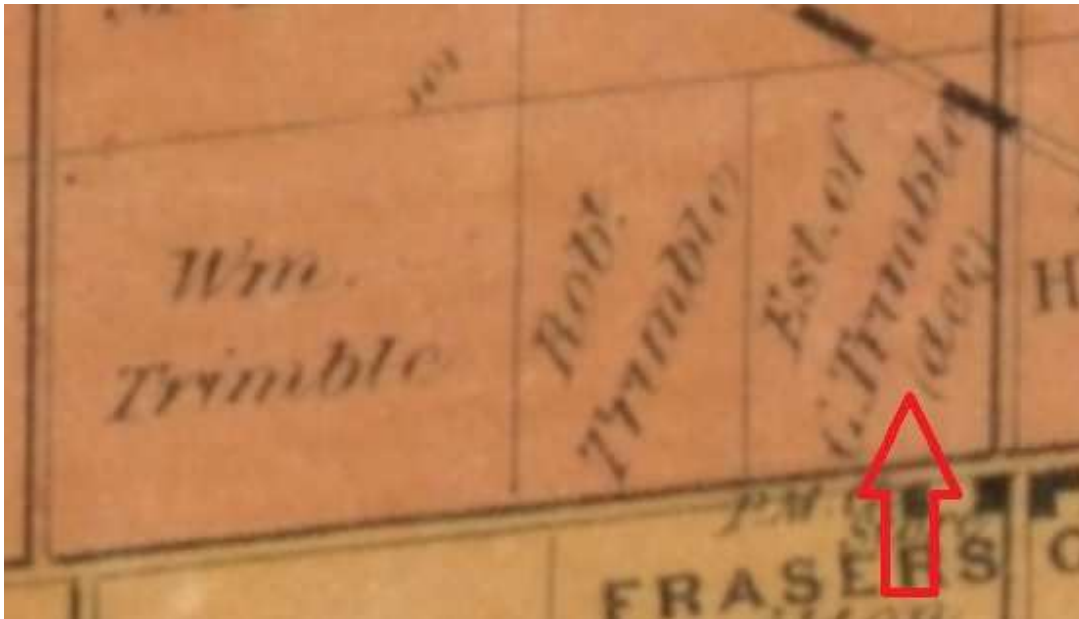


Figure 1: Detail of the 1859 Tremaine's Atlas of the County of Peel. Red arrow indicates the approximate location of 8000 Dixie Road

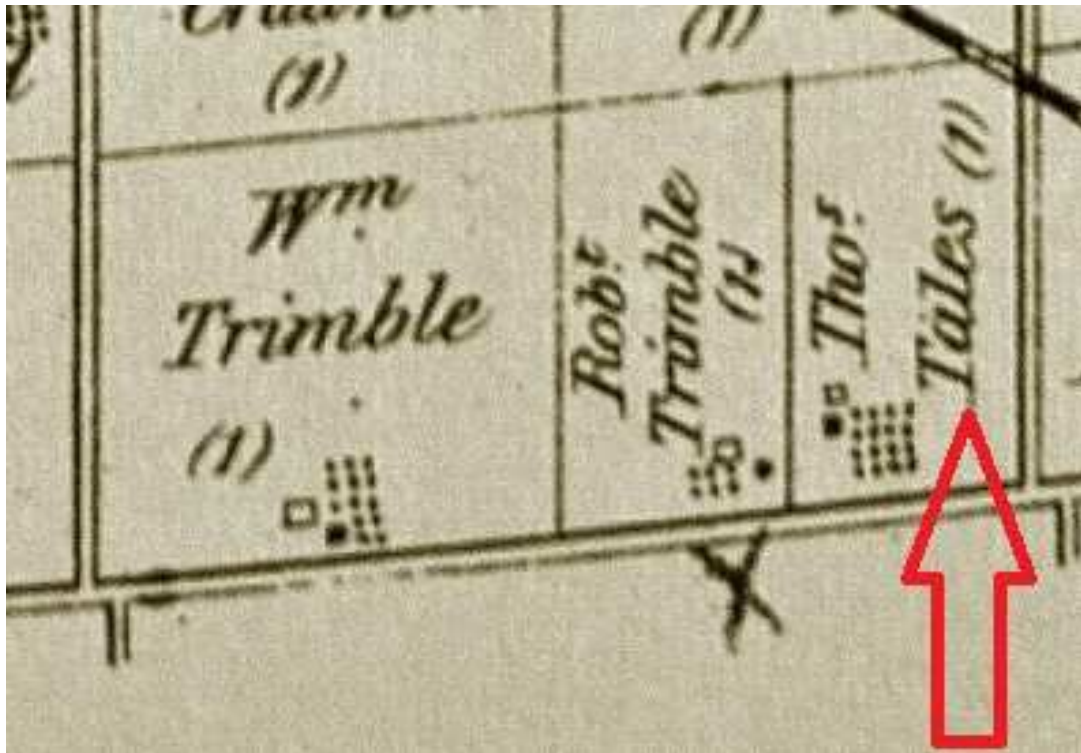


Figure 2: Detail of the 1877 Pope Atlas of Chinguacousy Township. Red arrow indicates the approximate location of 8000 Dixie Road.



Figure 3: Detail of 1909 topographic map of Chinguacousy Township. Red arrow indicates approximate location of 8000 Dixie Road.

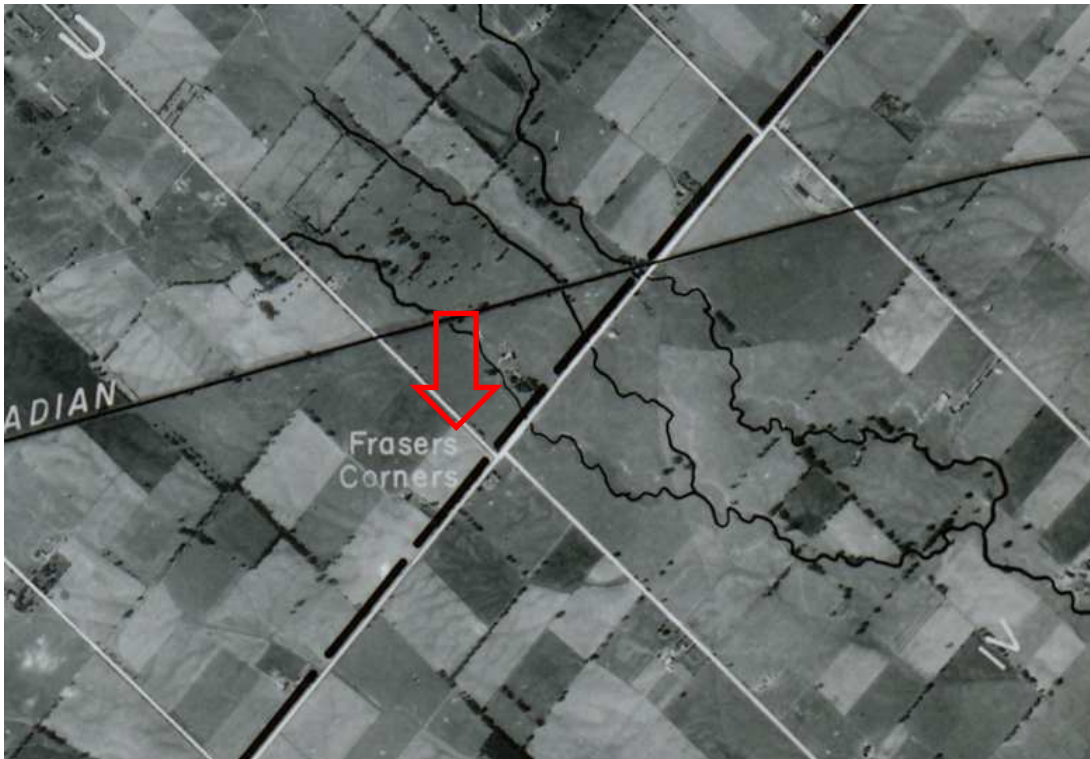


Figure 4: Portion of 1954 aerial image. Red arrow indicates location of Subject Property

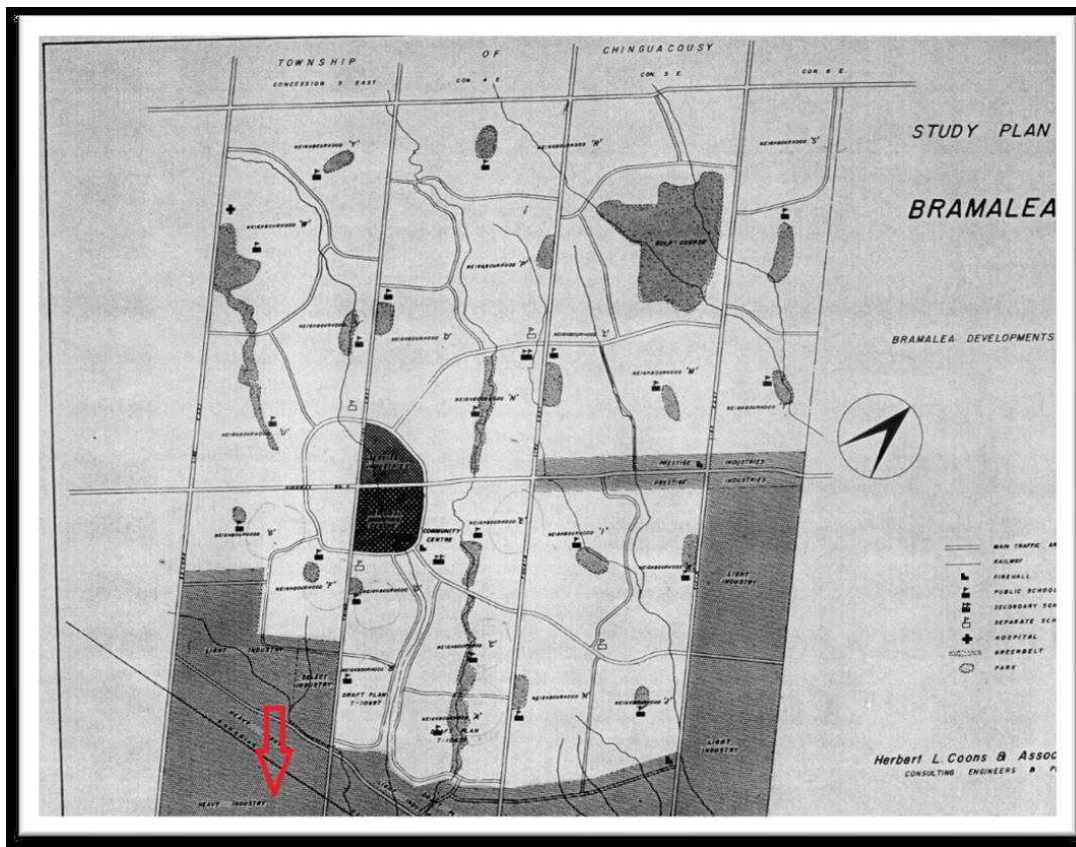


Figure 5: 1958 Master Plan of the City of Bramalea. The Subject Property's approximate location is indicated by the red arrow.



Figure 6: 1964 aerial image, structure is still under construction.



Figure 7: 1965 aerial image of Subject Property.



Figure 8: 1968 aerial image showing the Subject Property and the Ford Motor Company Bramalea Parts Distribution Centre.

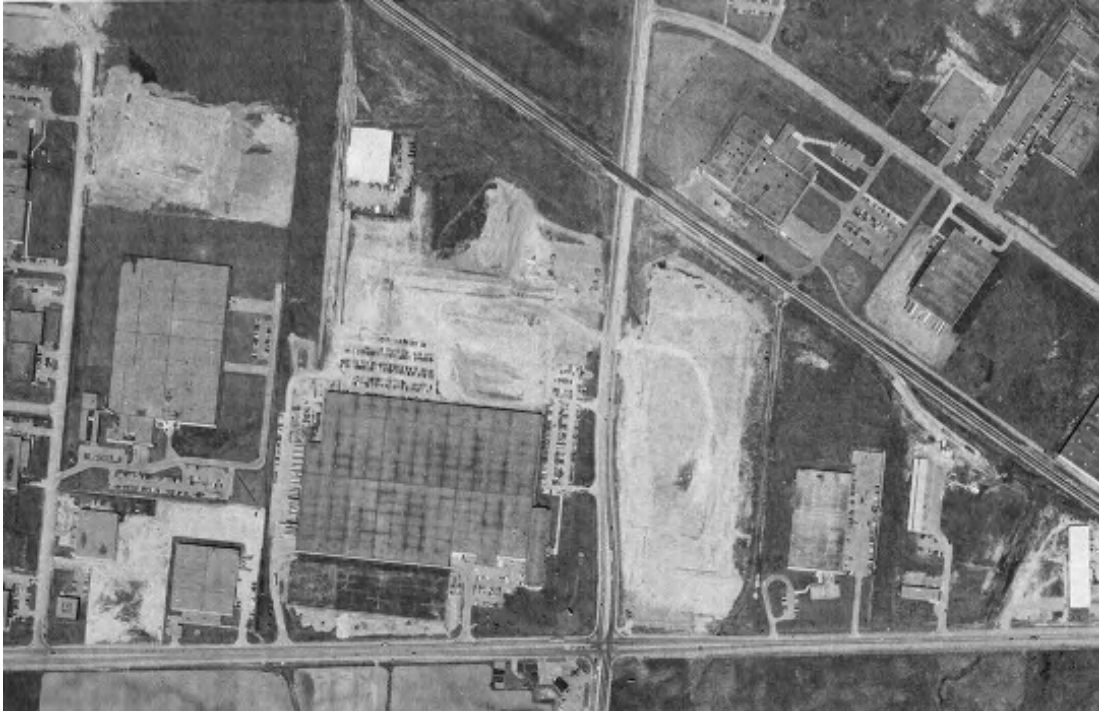


Figure 9: 1973 Aerial. Rear addition still not present.

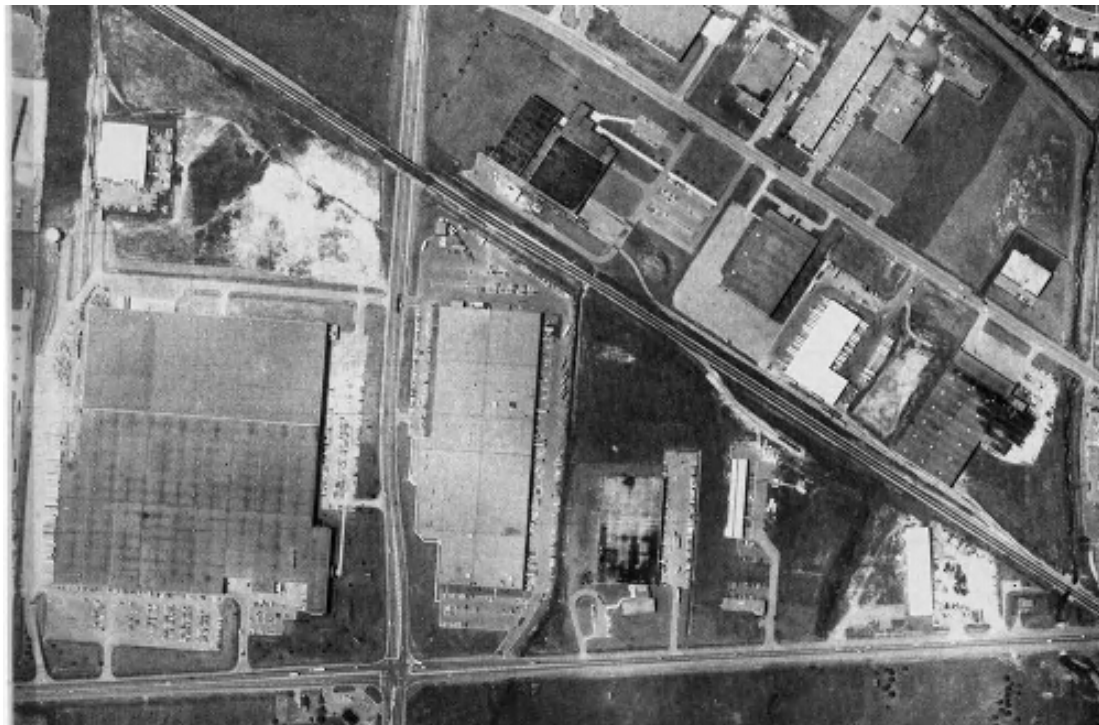


Figure 10: 1975 Aerial Image. Rear addition is now in place.



Figure 11: Undated photograph, showing the Ford Distribution Centre located at 8000 Dixie Road. The exterior remains largely unchanged from its initial construction.



Figure 12: Aerial view of Intersection of Steeles Avenue East and Dixie Road. Subject Property is on left of image.



Figure 13: 2019 Google Earth image of 8000 Dixie.

4.2.3 Chronological History of Development

Prior to the construction of the extant warehouse and administration building by Ford Motor Company of Canada the subject property was undeveloped agricultural land. The extant structure was constructed between 1963 and 1964. The structure underwent an addition in 1974 when a 300,000 square foot addition was added (Globe and Mail, 1972). The Administration complex and to a lesser extent the warehouse is largely unaltered from since construction. The complex reflects the International Style of architecture; a modernist movement popular in the mid-twentieth century (Blumenson, 1990). The international Style was derived as a way to showcase modern construction techniques and is defined by its rejection of architectural styles of the past. The international style was applied to a wide range of structures and vernacular application of it was easily applied to mid-20th century industrial structures. Attempts to obtain information regarding the construction and design of the building from Ford Canada did not yield results. Personal communication with Courtney McAlpine, Senior Digital Archivist, Ford Motor Co. was unable to find any documents pertaining to the construction and design of the facility. The same result was encountered when Nick Moreau, Archivist, Peel Art Gallery, Museum, and Archives was contacted.

4.2.4 Ownership History

Table 1: Land transaction history associated with 8000 Dixie Road.

| Inst. | Date | Grantor | Grantee | Comment |
|-------|----------------|-----------------------------|---|---|
| ----- | September 1836 | The Crown | Robert Trimble | Patent, All 200 acres |
| 17928 | March 1840 | Robert Trimble | ----- | Will |
| 20354 | June 1842 | Robert Trimble | William Trimble | Bargain and Sell, NW ¼ 50 acres |
| 49689 | April 1853 | George Trimble et ux | Toronto & Guelph Railway | Bargain and Sell, 64/100 acres |
| 4469 | February 1853 | George Trimble | ----- | Will |
| 1530 | May 1873 | Robert Trimble | Mary Ann Trimble | Will, E ½, NW ¼, SW ¼ |
| 2470 | March 1876 | William Trimble et al | Thomas Tales | Bargain and Sell, E ½ of E ½ |
| 3688 | May 1880 | Thomas Tales et ux | Robert Trimble | Bargain and Sell, part s ½, 1/5 |
| 5491 | November 1886 | Ebenezer Long & [Illegible] | Robert Laycock et al | Declaration, part S ½, 1/5 |
| 8581 | July 1886 | George Sinclair | James Sinclair | Assignment of Mortgage, W ½ of E ½, 50 acres |
| 8654 | May 1899 | Thomas Tales et ux | Thomas G. Baldock | Bargain & Sell, E ½ of E ½, 50 acres |
| 10177 | August 1906 | Robert Trimble (widower) | Thomas G. Baldock | Bargain & Sell, W ½ of E ½, 50 acres |
| 14902 | May 1923 | Martha E. Baldock | Cyril H. Middlebrook | Grant, E ½ 100 acres |
| 15960 | April 1927 | Cyril H. Middlebrook et ux | Alfred Cowton | Grant, E ½ except part |
| 16240 | April 1928 | Alfred Cowton et ux | Frederick Ellis | Grant, E ½ except part, Mortgage No. 16237 |
| 16651 | January 1930 | Alfred Cowton, et ux | Francis J. Jackson | Assignment of Mortgage, No. 16237 |
| 17969 | April 1935 | Frederick Ellis, et ux | Charles H. Bowyer exr of Alfred Cowton | Quitclaim, E ½ except 64/100 acres NE corner, No. 16237 |
| 18202 | June 1938 | Estate of Alfred Cowton | ----- | Caution, May be necessary to tell |
| 18224 | August 1938 | Annie M. & Francis Jackson | Charles H. Bowyer, exr of Alfred Cowton | Assignment of Mortgage, No. 16237 |

| | | | | |
|-------|----------------|---|---|--|
| 18225 | June 1938 | Charles H. Bowyer, exr of Alfred Cowton | Stella B. Middlebrook | Grant, Part E ½, 99 36/100 acres |
| 18351 | May 1939 | Stella B. Middlebrook | Thomas M. Robinson | Grant, E ½ except 64/100 acres |
| 19244 | March 1943 | Thomas M. Robinson | Lillian L. & Clarence E. Prosser, joint tenants | Grant, all E ½ SW of CNR (99.36 acres) |
| 19639 | May 1945 | Lillian L. & Clarence E. Prosser | John M. Moddison | Grant, all E ½ SW of CNR (99.36 acres) |
| 356 | August 1949 | Township of Chinguacousy | | By-Law, No. 743 re: subdivision control (all & O.L.) |
| 25510 | December 1957 | Thomas H. Robinson | Bayton Holdings, Ltd | Grant, Part E ½ & O.L. (100.691 ac) |
| 25496 | January 1958 | John M. Moddison | Bayton Holdings, Ltd. | Grant, E ½ SW of CNR & O.L. (105.48 acres) |
| 25508 | January 1958 | Cyril H. Middlebrook | Thomas M. Robinson | Grant, Part E ½, 0.323 ac |
| 26061 | March 1958 | Bayton Holdings, Ltd. | Bramalea Consolidated Developments, Ltd | Grant, E ½ SW of CNR & O.L. |
| 26063 | March 1958 | Bayton Holdings, Ltd | Bramalea Consolidated Developments, Ltd | Grant, E ½ & W ½, Mortgage in No. 25510 |
| 640 | September 1960 | Bramalea Consolidated Developments, Ltd. | | Plan, subdivide part E ½ & O.L. |
| 30475 | December 1961 | Bramalea Consolidated Developments, Ltd | Canadian National Railway Co | Grant, Parts & O.L., NW of E angle Lot 1 |
| 685 | March 1962 | Corporation of the Township of Chinguacousy | ----- | By-Law, to expropriate for 17' road widening b/t Con. 3 & 4 |
| 31025 | April 1962 | County of Peel | Bramalea Consolidated Developments, Ltd. | Notice of Expropriation, et al & O.L., req. for public purposes |
| 676 | July 1962 | Bramalea Consolidated Developments, Ltd. | | Plan, Sub. Part & O.L. |
| 31917 | March 1963 | Bramalea Consolidated Developments, Ltd. | Ford Motor Company of Canada | Grant, Part 60 acres (?) |
| 33357 | May 1963 | County of Peel, re: 3 rd Line East | Ford Motor Company of Canada, Ltd | Notice of Expiration, 0.091 acres |
| 33392 | January 1964 | The Clarkson Company Ltd. | Ford Motor Company of Canada, Ltd | Mechanic's Lien, part as in No. 31917, except 0.091 acres in No. 33357 |

| | | | | |
|----------|-----------------|---|---|---|
| 34304 | October 1964 | Ford Motor Company of Canada, Ltd. | The Corporation of the County of Peel | Grant, Part 0.091 acres |
| 34501 | August 1964 | County of Peel | Ford Motor Co. | Transfer Easement, re: purposes of [Illegible] |
| VS145461 | July 1970 | Township of Chinguacousy | ----- | By-Law No. 118-70 subsection 4 of Planning Act does not apply to All & O.L. |
| VS200332 | February 1972 | ----- | Ford Motor Company of Canada | Transfer |
| VS291820 | November 1973 | Petrison Contracting, Ltd | Ford Motor Company of Canada & The Frid Construction Co Ltd | Mechanic's Lien, Part as in No. 31917 (60 acres), except 0.091 acres as in 33357 |
| VS297588 | January 1974 | Petrison Contracting, Ltd (Pltf) | Ford Motor Company of Canada, The Frid Construction Co, Ltd (Def) | Certificate, To comm. action re: lien for part as in No. 31917, except for 0.91 acres as in 33357 |
| VS300690 | February 1974 | Petrison Contraction, Ltd (Pltf) | Ford Motor Company of Canada, The Frid Construction Co, Ltd (Def) | Order, To vacate No. VS297588 |
| VS309273 | April 1974 | Sayers & Associates, Ltd | Ford Motor Company of Canada, Ltd | Mechanic's Lien, Part as in No. 31974 |
| VS319549 | June 1974 | Sayers & Associates, Ltd (Pltf) | Ford Motor Company of Canada, The Frid Construction Co, Ltd (Def) | Certificate, To commence action re: lien for 153,111.79, re: 31974 |
| VS332845 | October 1974 | Sayers & Associates Ltd (Pltf) | Ford Motor Company of Canada, The Frid Construction Co, Ltd (Def) | Certificate, to vacate Nos. 309273VS & 319549VS |
| 43R2804 | January 1975 | Plan Reference 43R-2804, Part 1 (0.028 acres) | | |
| VS351950 | March 1975 | The Regional Municipality of Peel | Ford Motor Company of Canada | Quitclaim, Part designated as Part 1 on 43R-2804 |
| 43R5755 | 1978? [Missing] | Plan Reference 43R-5755, Part 1 & O.L. | | |
| 495418 | September 1978 | Ford Motor Company of Canada Ltd | Regional Municipality of Peel | Grant, Part on widening & O.L. designated as Part 1 on 43R-5755 |
| 43R9327 | September 1981 | Plan Reference 43R-9327, Part 1 (0.346 ha + O.L.) | | |

| | | | | |
|-----------|---------------|---|-----------------------------------|---|
| RO604217 | March 1982 | Ford Motor Company of Canada | The Regional Municipality of Peel | Transfer Easement, Part designated as Parts 1, 2 & 3 on 43R-9327 |
| LT2057426 | March 2000 | HM The Queen, through Dept. of Transport Canada | ----- | Notice, Pearson Airport zoning regulation |
| PR861540 | June 2005 | Corporation of the City of Brampton | ----- | By-Law 143-2005 to repeal by-law exempting land from part lot control |
| PR863157 | June 2005 | The Regional Municipality of Peel | | Notice |
| PR1176597 | November 2006 | Ford Motor Company of Canada | Ford Motor Company | Charge |
| PR1185497 | December 2006 | Ford Motor Company of Canada | Ford Motor Company | Charge |
| 43R-31229 | December 2006 | Plan Reference 43R-31229 | | |
| PR1430782 | March 2008 | Ford Motor Company of Canada | The Regional Municipality of Peel | Transfer |
| PR1430783 | March 2008 | Ford Motor Company of Canada | The Regional Municipality of Peel | Transfer Easement |
| PR1430784 | March 2008 | Ford Motor Company | The Regional Municipality of Peel | Postponement, PR1176597 to PR 1430783 |
| PR1430785 | March 2008 | Ford Motor Company | The Regional Municipality of Peel | Postponement, PR1185497 to PR 1430783 |
| PR1430789 | March 2008 | Ford Motor Company | ----- | Discharge of Charge, see PR1176597 |
| PR1430787 | March 2008 | Ford Motor Company | ----- | Discharge of Charge, see PR1185497 |

5. Legislative and Policy Framework

The following assessment reviews provincial and municipal legislation and policies designed to protect cultural heritage resources that may be affected by development in the City of Brampton. This HIA has been prepared to meet the terms of reference set forth by the *Ontario Heritage Act*, the *Planning Act*, the *Provincial Policy Statement*, the *City of Brampton Official Plan*, as described in the 2020 Terms of Reference for Heritage Impact Assessments (Milton, 2020).

5.1 Provincial Legislation and Policy

5.1.1 Ontario Heritage Act

The criteria for determining cultural heritage value or interest are outlined in the *Ontario Heritage Act* (OHA) under Regulation 9/06. (1) The criteria set out in subsection (2) are prescribed for the purposes of Clause 29 (1) (a) of the Act. (2) A property may be designated under Section 29 of the Act if it meets one or more of the following criteria for determining whether it is of cultural value or interest:

1. The property has design value or physical value because it:
 - a. is a rare, unique representation or early example of a style, type, expression, material, or construction method,
 - b. displays a high degree of craftsmanship or artistic merit, or
 - c. demonstrates a high degree of technical or scientific achievement.
2. The property has historical value or associative value because it:
 - a. has direct associations with a theme, event, belief, person, activity, organization, or institution that is significant to a community,
 - b. yields, or has the potential to yield, information that contributes to an understanding of a community or culture, or
 - c. demonstrates or reflects the work or ideas of an architect, artist, builder, designer, or theorist who is significant to the community.
3. The property has contextual value because it:
 - a. is important in defining, maintaining, or supporting the character of an area,
 - b. is physically, functionally, visually, or historically linked to its surroundings, or
 - c. is a landmark.

5.1.2 Planning Act

The *Planning Act* (1990) provides the legislative framework for land use planning in Ontario. Part 1, Section 2(d) and (r) of the Act identifies matters of provincial interest.

Part I, Section 2

The Minister, the council of a municipality, a local board, a planning board and the Tribunal, in carrying out their responsibilities under this Act, shall have regard to, among other matters, matters of provincial interest such as

(d) the conservation of features of significant architectural, cultural, historical, archaeological, or scientific interest,

(e) the promotion of built form that,

i. is well designed

ii. encourages a sense of place, and

iii. provides for public spaces that are of high quality, safe, accessible, attractive, and vibrant.

5.1.3 Provincial Policy Statement (2020)

The Provincial Policy Statement (PPS), issued under Section 3 of the *Planning Act*, came into effect on May 1, 2020. It applies to all planning decisions made on or after that date and replaced the PPS, 2014. The PPS provides direction for the appropriate regulation for land use and development while protecting resources of provincial interest, and the quality of the natural and built environment, which includes cultural heritage archaeological resources. These policies are specifically addressed in Part V, Sections 1.7 and 2.6.

Section 1.7.1e of the PPS addresses long-term economic prosperity by “encouraging a sense of place, by promoting well-designed built form and cultural planning, and by conserving features that help define character, including built heritage resources and cultural heritage landscapes.”

Section 2.6 of the PPS addresses the protection and conservation of cultural heritage and archaeological resources in land use planning and development and requires the following:

2.6.1 Significant built heritage resources and significant cultural heritage landscapes shall be conserved.

2.6.2 Development of site alteration shall not be permitted on lands containing archaeological resources or areas of archaeological potential unless significant archaeological resources have been conserved.

2.6.3 Planning authorities shall not permit development and site alteration on adjacent lands to protected heritage property except where the proposed development and site alteration has been evaluated and it has been demonstrated that the heritage attributes of the protected heritage property will be conserved.

2.6.4 Planning authorities should consider and promote archaeological management plans and cultural plans in conserving cultural heritage and archaeological resources.

2.6.5 Planning authorities shall engage with Indigenous communities and consider their interests when identifying, protecting, and managing cultural heritage and archaeological resources.

5.2 Municipal Policy Framework

5.2.1 City of Brampton Official Plan

The *City of Brampton Official Plan* was first adopted in 2006 and was most recently consolidated in September 2020 (City of Brampton, 2006). The Official Plan states that Cultural Heritage will be preserved, and Section 4.10 of the Official Plan specifically addresses cultural heritage resources and outlines the City's cultural heritage resource policies:

- ▶ Conserve the cultural heritage resources of the City for the enjoyment of existing and future generations,
- ▶ Preserve, restore, and rehabilitate structures, buildings, or sites deemed to have significant historic, archaeological, architectural, or cultural significance, and preserve cultural heritage landscapes, including significant public views, and,
- ▶ Promote public awareness of Brampton's heritage and involve the public in heritage resource decisions affecting the municipality.

Section of the Official Plan addresses the preferred hierarchy of options to conserve Cultural Heritage Resources, stating that:

All options for the on-site retention of properties of cultural heritage significance shall be exhausted before resorting to relocation. The following alternatives shall be given due consideration in the order of priority:

- i. On-site retention in the original use and integration with the surrounding or new development;*
- ii. On-site retention in an adaptive re-use;*
- iii. Relocation to another site within the same development, and*
- iv. Relocation to a sympathetic site within the city.*

The full text of the official plan is available on the City's webpage.

5.2.2 City of Brampton Heritage Impact Assessment Terms of Reference 2020

Heritage Impact Assessments in the City of Brampton are conducted under the standards set forth by their terms of reference adopted in 2020 (Brampton, 2020). A PDF of the TOR is available on the City of Brampton's webpage.

6. Evaluation of Cultural Heritage Value or Interest

During the property visit the Subject Property and surrounding area was photo documented. Attention was paid to documenting the views both into and out of the Subject Property.

6.1.1 Surrounding Area

The Subject Property is located at the intersection of Dixie Road and Steels Avenue East. The area is dominated by industrial activities including warehouse and industrial manufacturing. The surrounding area contains major transportation corridors including Highway 410 to the west, Highway 407 to the southeast, and a rail corridor to the north. The area was originally part of planned community of Bramalea, Canada's first planned satellite community. Bramalea was later amalgamated into the City of Brampton.



Figure 14: View from second floor of administration building, looking east towards Dixie Road.



Figure 15: View from second floor of administration building, facing west. Steeles Avenue East is on the far left of image.

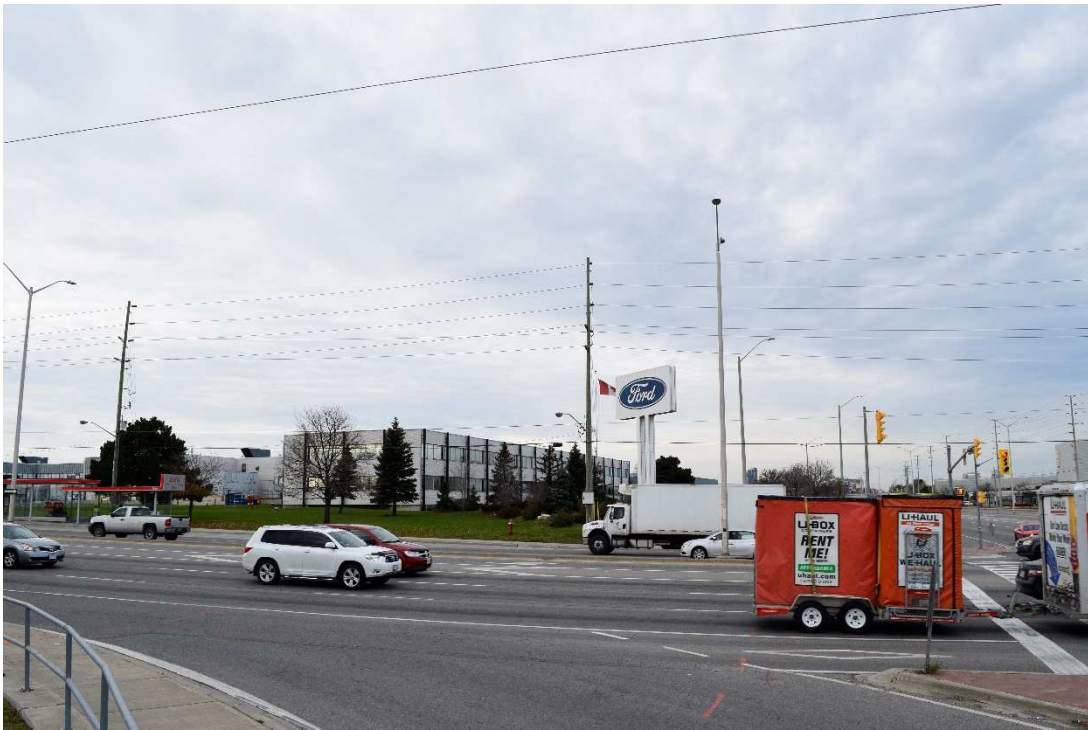


Figure 16: Corner of Steeles Avenue East and Dixie Road. Facing northwest.

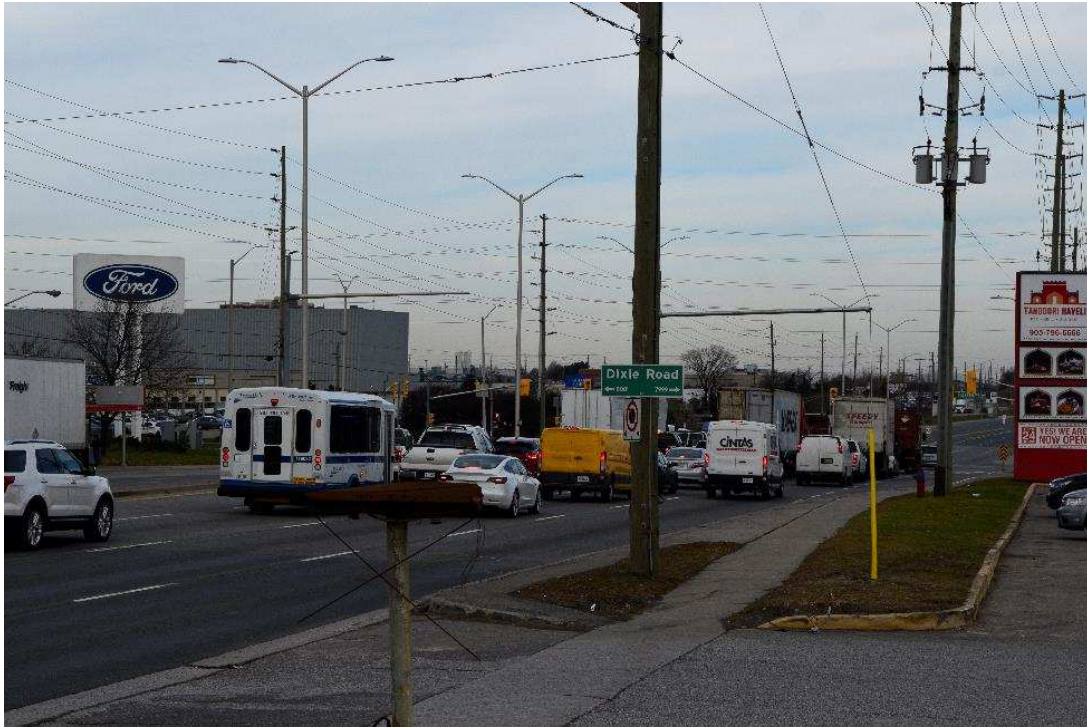


Figure 17: Intersection of Steeles Avenue East and Dixie Road. Facing northeast.



Figure 18: View towards Steeles Avenue East from main entrance of administration building.



Figure 19: Looking north parallel to Dixie Road from main entrance of administration building.



Figure 20: Panoramic view from main entrance to administration building. Facing north towards Dixie Road.

6.2 Ford Motor Company Parts Distribution Centre

The structure is common industrial construction. Utilizing a structural frame of steel “I beams” with the exterior clad in smooth white brick. The brick is set in a common bond pattern suggesting the outer skin of the structure is of double brick construction indicating it is an integral part of the overall structure. The warehouse is ringed by a course of glazing permitting natural light to enter the structure while not impeding the use of wall surfaces for loading bays. The administration building is of the same overall construction as the warehouse displaying an internal structure comprised of steel “I beams” skinned in a double layer of smooth white brick. The exterior of the administration center is augmented by the application of a series of nonstructural steel “I beams”. The external “I beams” are bolted to the exterior of the building and rest on small circular concrete pads. Major structural divides within the administration building are of terracotta block. The interior of the administration building presents largely as built in 1963. The second floor is currently vacant, serving as a storage space for excess office furniture and antiquated electronics. The first floor remains in use and retains its as-built configuration. A large centrally located spiral staircase serves as the primary means of egress within the administration building. The staircase is constructed of polished concrete with a metal railing supporting a wood handrail.

6.2.1 Exterior



Figure 21: Southern corner of warehouse, looking north towards Dixie Road.



Figure 22: Frontage of warehouse onto Steeles Avenue East. Facing southwest.



Figure 23: Junction between administration building and warehouse. Facing north.



Figure 24: Southeast face of administration building. Facing northwest.



Figure 25: Southeast corner of administration building, Dixie Road facing façade.



Figure 26: Dixie Road façade of administration building. Facing south.



Figure 27: Northeast corner of administration building. Facing south.



Figure 28: Junction between administration building and on east side of structure. Facing south.



Figure 29: East side of warehouse. Change in elevation indicates connection between 1973 addition and original structure. Facing northwest.



Figure 30: Northeast corner of warehouse. Facing south.



Figure 31: North wall of warehouse. Facing southwest.



Figure 32: Parking on north wall of warehouse. Facing southwest.



Figure 33: Main entrance doors



Figure 34: Close up of cement structure comprising the elevated main entrance.



Figure 35: Applied steel framing, used to achieve character defining vertical separations in façade. Framing is painted black to contrast with smooth white brick.



Figure 36: Existing Ford signage located at the corner of Steeles Avenue East and Dixie Road, facing north



Figure 37: Existing Ford signage located at the corner of Steeles Avenue East and Dixie Road, facing south

6.2.2 Interior

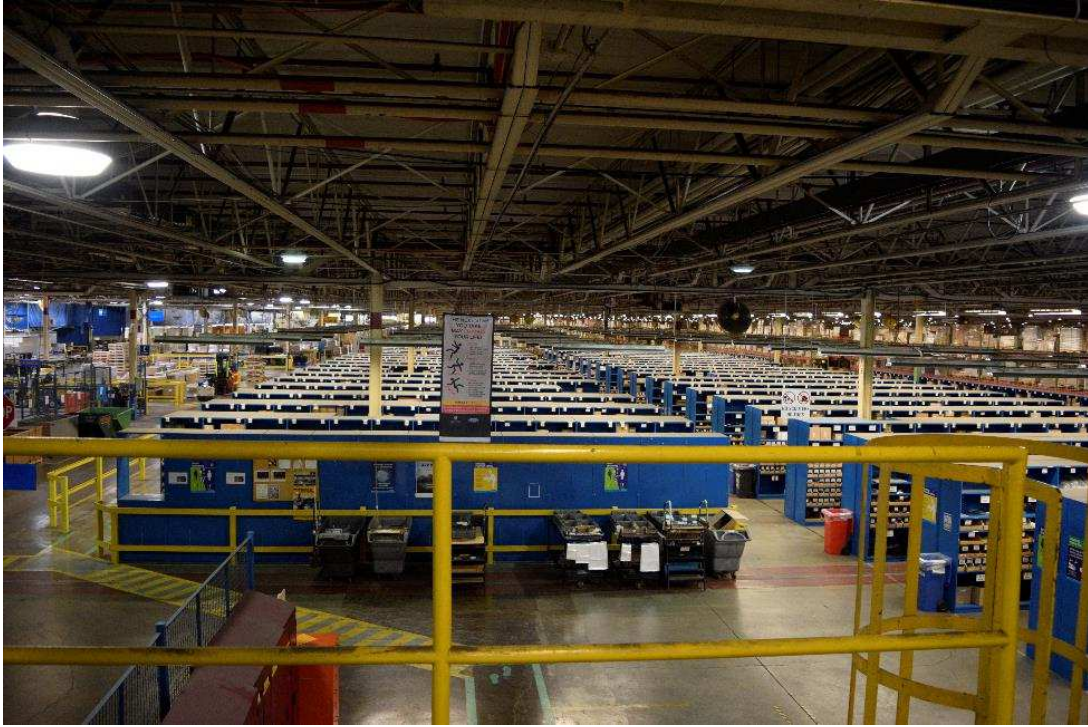


Figure 38: Interior of warehouse. Facing southwest.

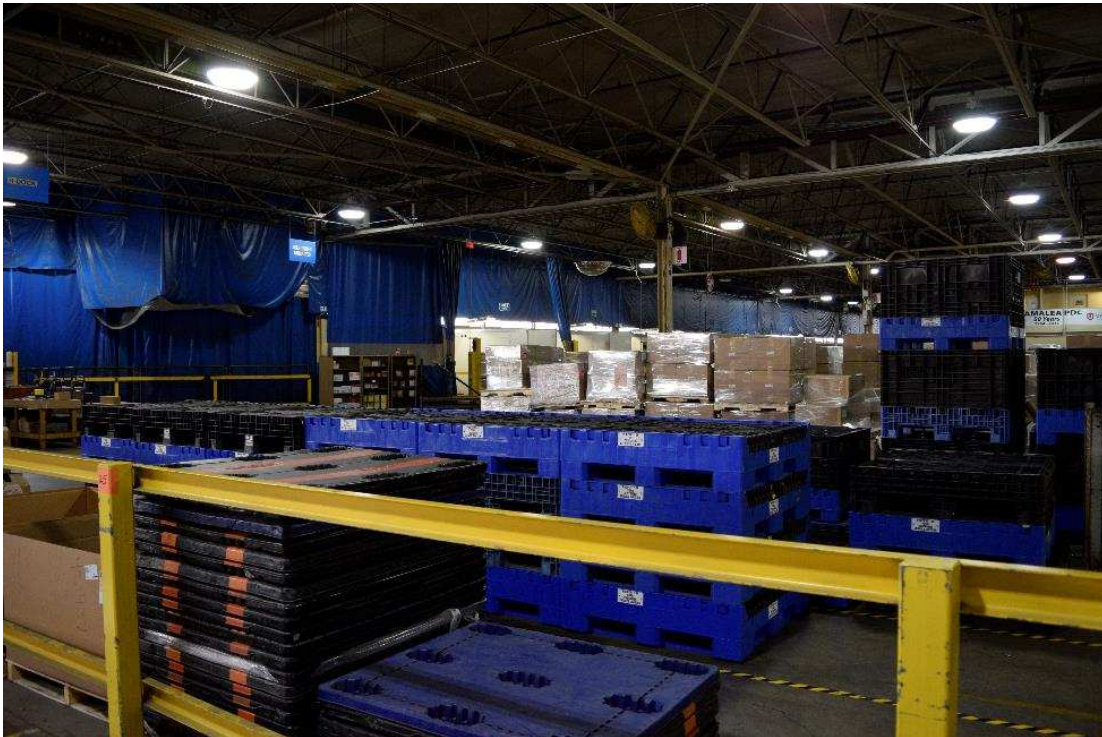


Figure 39: Typical view of interior of warehouse.

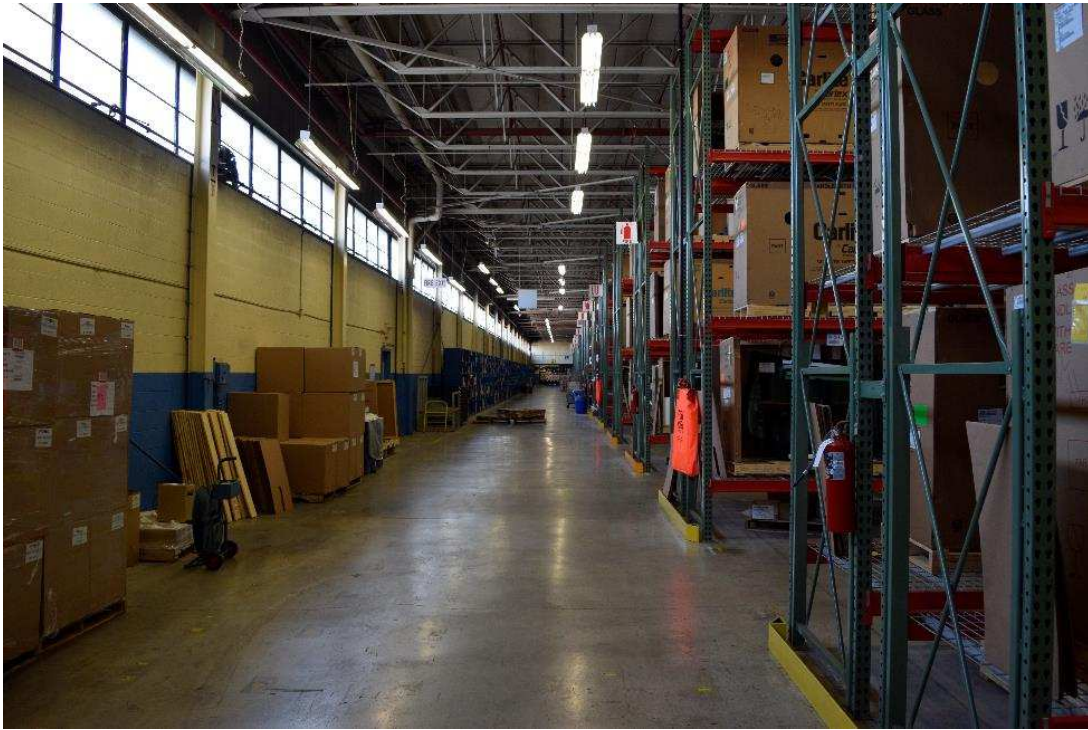


Figure 40: Interior of warehouse, west wall.



Figure 41: Mechanical room in warehouse.



Figure 42: Document storage. Note pneumatic tools on ceiling.



Figure 43: Interior of north end of second floor administration building.



Figure 44: Pneumatic tube access point on second floor of administration building.



Figure 45: Typical cubical configuration, second floor administration building.



Figure 46: Closeup of cubical configuration, second floor administration building.



Figure 47: “Corner office” northeast corner of second floor administration building.



Figure 48: Typical office space.



Figure 49: Internal sitting room, second floor of administration building



Figure 50: Board room, Second floor administration building. Facing east.



Figure 51: Board room, Second floor administration building. Facing west.



Figure 52: Overview of office space in south end of administration building.



Figure 53: Boardroom first floor of administration building.



Figure 54: Office space on east side of administration building.



Figure 55: Mechanical/tornado safety room, Basement of administration building



Figure 56: Typical hall, Lower level administration building.



Figure 57: Cafeteria, Southeast corner of lower level of administration building. Facing east.



Figure 58: Cafeteria, Southeast corner of lower level of administration building. Facing west.

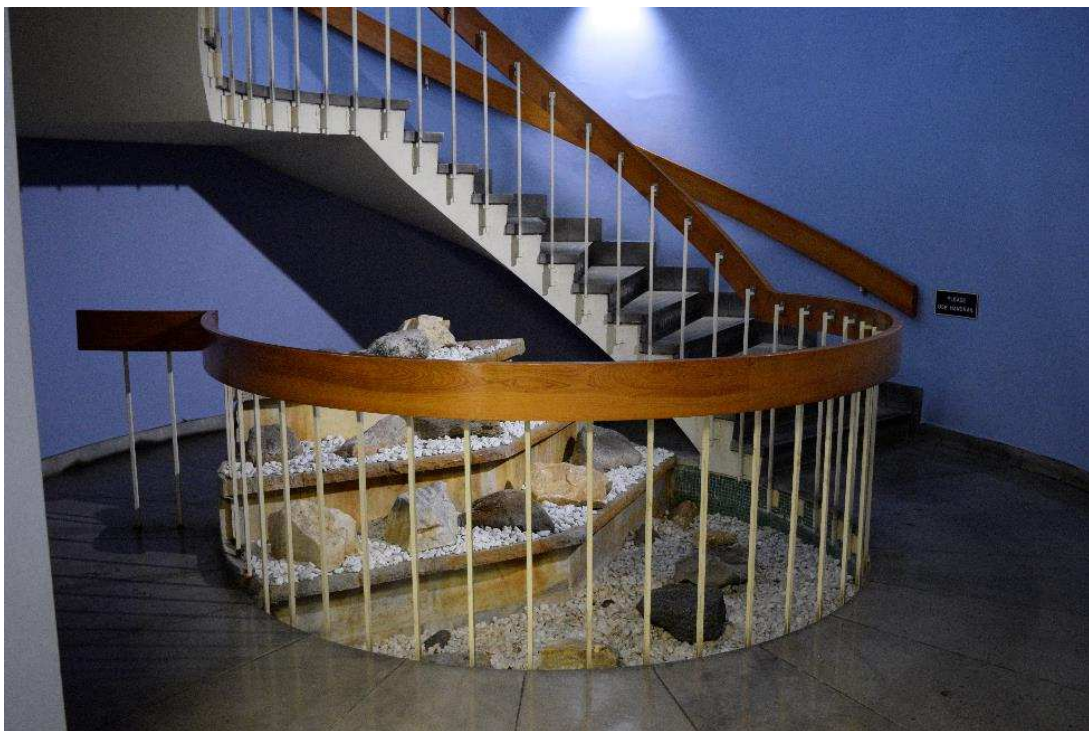


Figure 59: Main staircase administration building. Landing in lower level.



Figure 60: Lighting in main staircase of administration building.



Figure 61: Art installation within main staircase, lower level.

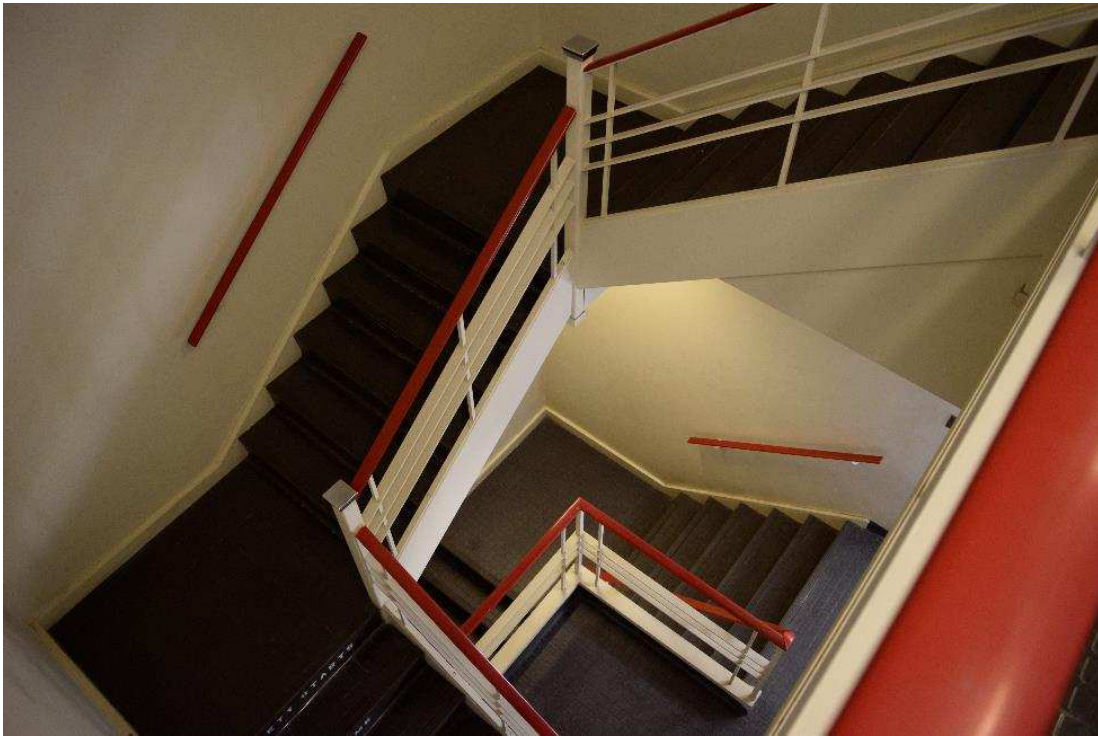


Figure 62: Typical secondary staircase of administration building.

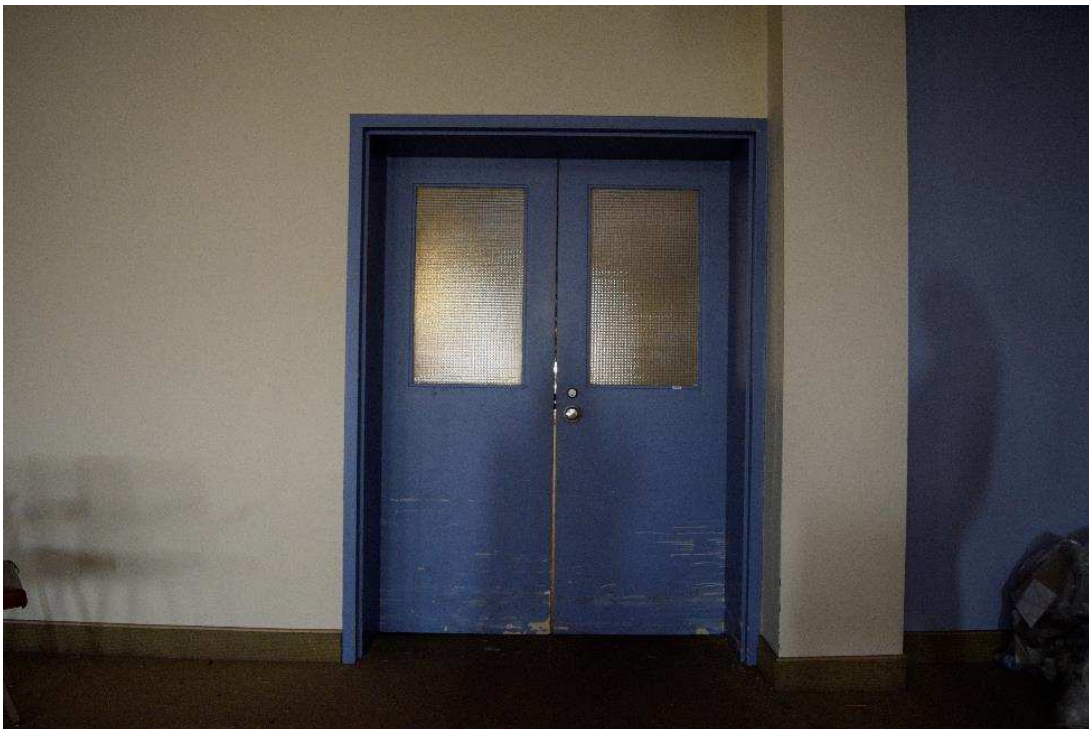


Figure 63: Example of typical doors with reenforced privacy glass.



Figure 64: Example of ceiling mounted TVs located throughout administration building.



Figure 65: Example of ceiling mounted clocks located throughout administration building



Figure 66: Integrated wall clock located in board rooms of administration building.



Figure 67: Example of terracotta structural block used to construct administration building.



Figure 68: Example of construction techniques employed in administration building.



Figure 69: Typical "I" beam framing as seen in administration building.

6.3 Floor Plans

Schematics of the structure were located on each floor and provide a basic understanding of the overall design of the structure.

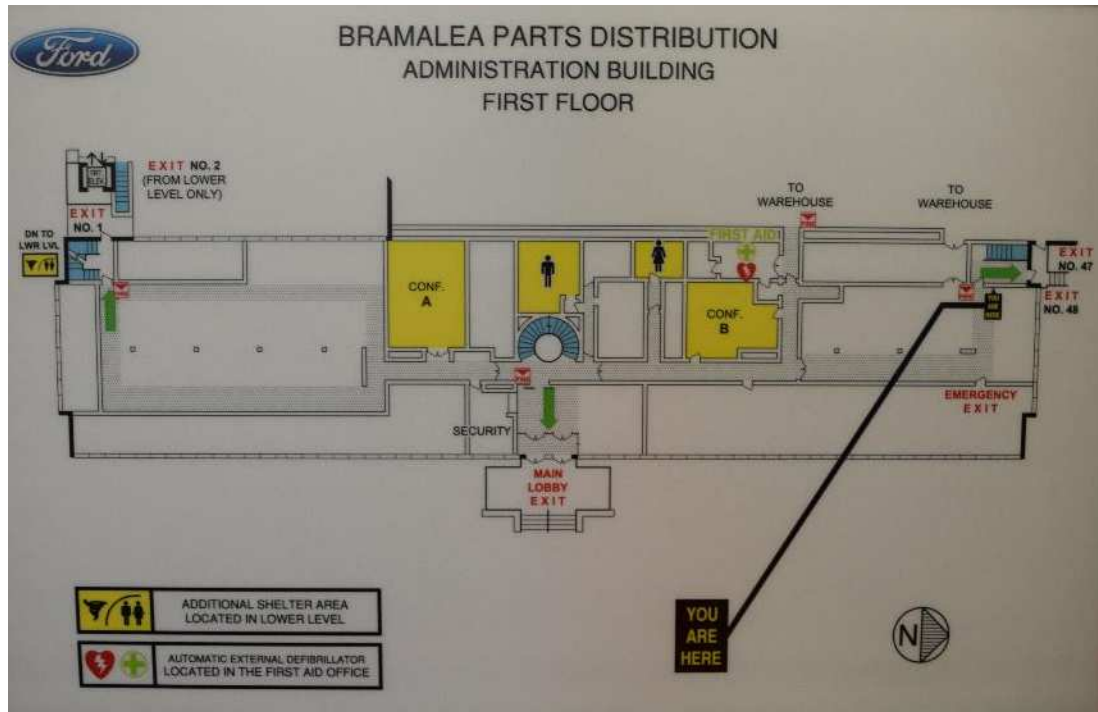


Figure 70: First floor of administration building

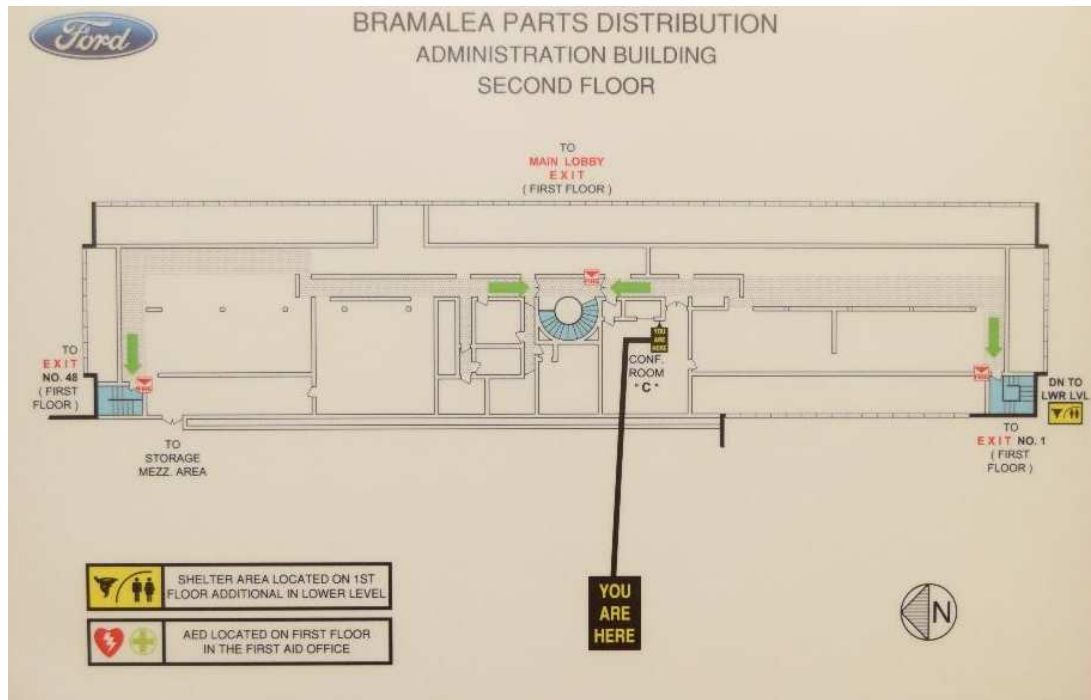


Figure 71: Second floor of administration building

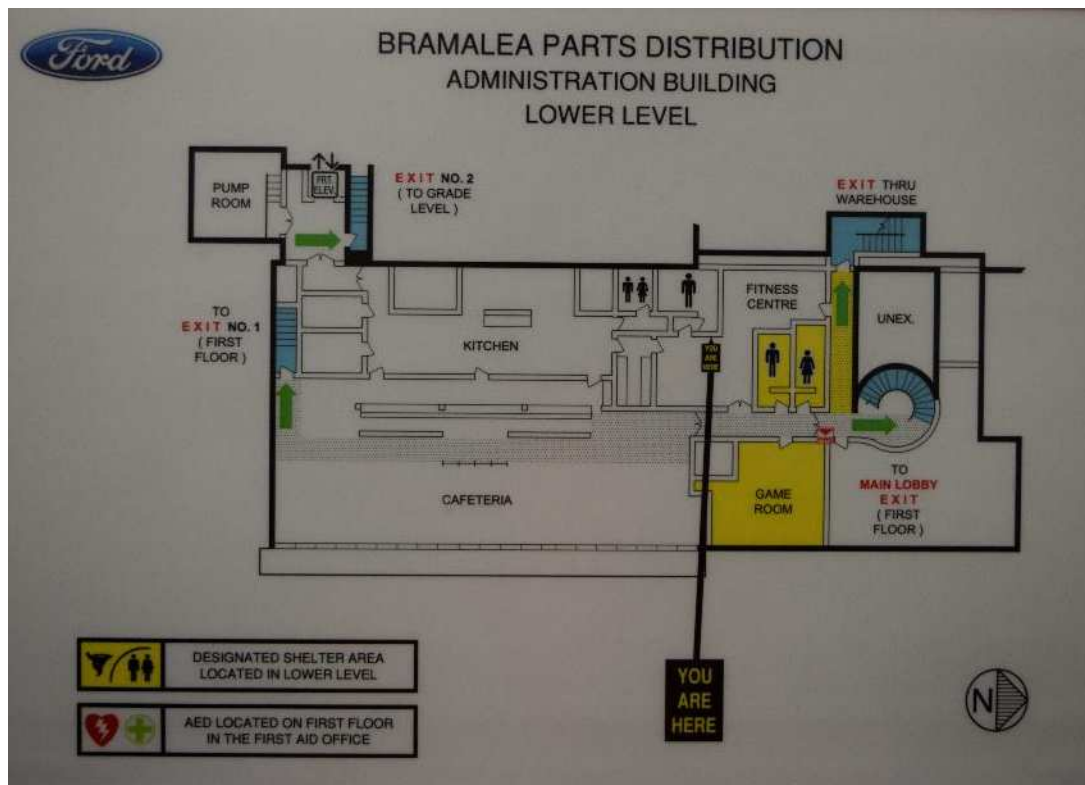


Figure 72: Lower level of administration building

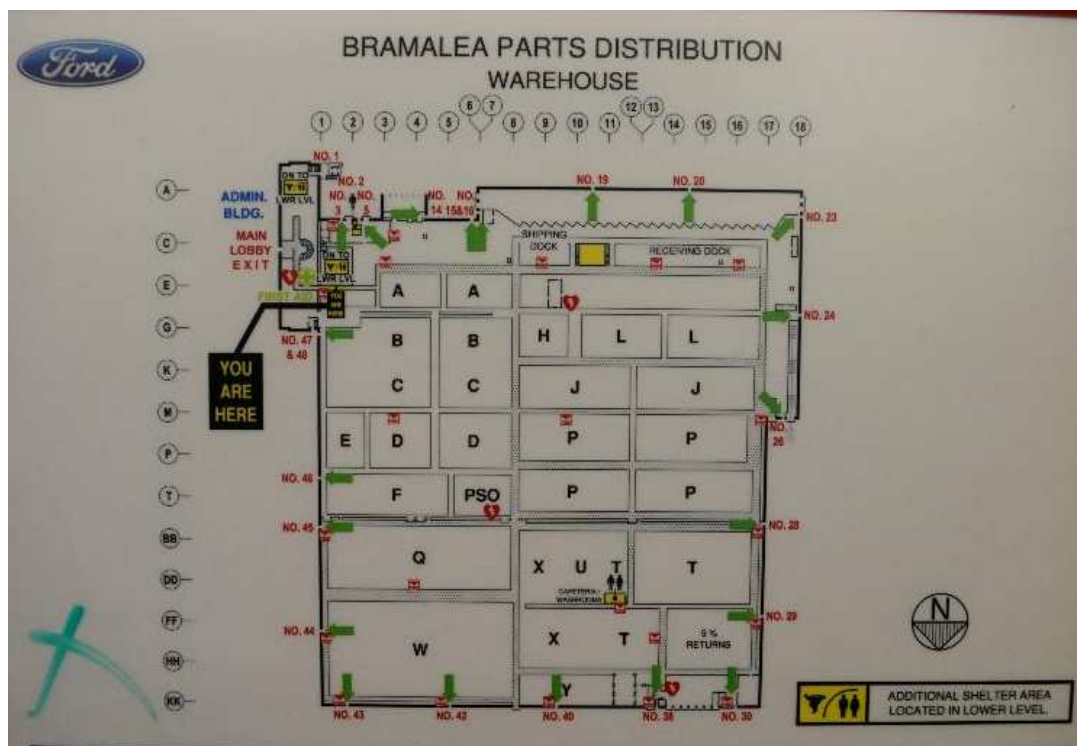


Figure 73: Full complex, Administration building in top left of image.

6.4 Criteria for Determined Cultural Heritage Value or Interest

Ontario Regulation 9/06 prescribes the criteria for determining the Cultural Heritage Value or Interest (CHVI) of a property. The regulation requires that, to be designated, a property must meet “one or more” of the criteria grouped into the categories of Design/Physical Value, Historical/Associative Value and Contextual Value (MHSTCI 2006a). Table 2 lists these criteria and identifies if the criteria was met at 8000 Dixie Road; these criteria categories are expanded on below.

Table 2: Ontario Regulation 9/06 Evaluation

| O.Reg.9/06 Criteria | Criteria Met (Y/N) | Justification |
|---|--------------------|--|
| The property has design value or physical value because it, | | |
| I. is a rare, unique, representative or early example of a style, type, expression, material, or construction method, | Y | The structure is reflective of the International architectural style. A popular mid-20 th century architectural style. Widely applied to industrial structures in the 20 th century. |
| II. displays a high degree of craftsmanship or artistic merit, or | N | None observed. |
| III. demonstrates a high degree of technical or scientific achievement. | N | None observed. |

| The property has historical value or associative value because it, | | |
|---|---|--|
| I. has direct associations with a theme, event, belief, person, activity, organization or institution that is significant to a community, | Y | Has direct association with the development of Bramalea. The Ford distribution centre was key to the economic viability of Bramalea |
| II. yields, or has the potential to yield, information that contributes to an understanding of a community or culture, or | N | The property and associated structure do not present with the potential to yield information that could contribute to our understanding of a community or culture. |
| III. Demonstrates or reflects the work or ideas of an architect, artist, builder, designer or theorist who is significant to a community. | N | None observed. Architect is unknown. |
| The property has contextual value because it, | | |
| I. is important in defining, maintaining or supporting the character of an area, | N | Not observed |
| II. is physically, functionally, visually or historically linked to its surroundings, or | Y | Mid-20 th century industrial complex instrumental in the success of the planned urban community of Bramalea |
| III. is a landmark. | N | The structure is not a local landmark. |

6.4.1 Design Value or Physical Value

The administration building and warehouse are an example of the industrial application the International Style. The international style was popular between 1930 and 1965 and was widely applied to industrial scale structures. 8000 Dixie represents a common application of the design, the most identifiable feature being the exposed steel framing, painted black juxtaposed against the smooth white brick of the administration building.

6.4.2 Historic Value or Associative Value

The historic value of the structure lies in the impact development of the property by Ford Motor Company played in the establishment of Bramalea. Ford was recruited to the area by the developers of Bramalea to serve as a key employer of potential residents.

6.4.3 Contextual Value

The Subject Property and Ford Motor Company were instrumental in the initial industrial development of Bramalea. The Ford Motor Company agreed to establish a parts distribution warehouse in Bramalea and was key to the success of the development of the community.

6.5 Defining Characteristics

- ▶ Exposed “I-Beam” frame
- ▶ Lack of ornamentation (minimalism)
- ▶ Horizontal runs of smooth brick and glazing (windows)
- ▶ Right angles

- ▶ Deep street setback
- ▶ Expansive manicured lawn with minimal landscaping

7. Proposed Development

Ford has opted to close the Bramalea parts distribution centre as of March 2023, opting to construct several smaller parts distribution nodes throughout southern Ontario. Ford has sold the property, and the new owners are seeking to redevelop the property into a series of smaller mixed use industrial warehouses and sales spaces. The proposed redevelopment outlines the demolition of the extant warehouse and administration building and calls for the construction of four freestanding structures to be located between the existing rail line, Dixie Road and Steeles Avenue East. The proposed re-development will increase the usable floor space of the property from 924,262 square feet to 1,286,762 square feet: an increase of 362,500 square feet. The proposed design includes an option for further expansion totaling 27,297 square feet. If fully recognized the re-development as proposed will increase the usable space on the property by 389,797 square feet.

In addition to the four industrial structures the proposed re-development would increase available parking space and provide for the construction of structures whose interior space could provide flexibility of design based on tenant needs.

Available schematics of the proposed re-development are presented in Appendix B.

7.1 Heritage Impacts

The proposed re-development of 8000 Dixie would result in the demolition of both the current warehouse and administration buildings.

While 8000 Dixie Road retains CHVI it does not warrant heritage designation under Part IV of the OHA. The extant mid-20th century two-storey administration building constructed of smooth white brick with exposed steel frame is a common example of the industrial application of the International Style. The style was nearing the end of its popularity in 1963 and was widely applied to industrial developments within Bramalea and the surrounding area.

The true value of 8000 Dixie lies in its contextual value and the contribution that the Ford Motor Company had to the development of Bramalea; a value that is not confined to its architectural style.

To further assess the potential heritage impacts of the proposed development The Ministry of Heritage, Sport, Tourism, and Culture Industries (MHSTCI) *Info Sheet #5 Heritage Impact Assessments and Conservation Plans* was reviewed and applied to the property (MHSTCI 2006b):

Destruction of any, or part of any, significant heritage attributes or features

- ▶ Demolition of the extant structure will result in the loss of identified defining characteristic (Section 6.5)

Alteration that is not sympathetic, or incompatible, with the historic fabric and appearance.

- ▶ Not applicable. No alterations are being considered at this time.

Shadows created that alter the viability of a heritage attribute or an associated natural feature or plantings, such as a garden.

- ▶ Proposed Re-development is not projected to exceed the elevation of the current structure as such there will be no detrimental change in shadow.

Isolation of a heritage attribute from its surrounding environment, context, or a significant relationship.

- ▶ The proposed re-development will not result in the isolation of heritage attributes from the surrounding area.

Direct or indirect obstruction of significant views or vistas within, for or of built and natural features.

- ▶ The proposed development will nor result in any new obstructions to views or vistas into or out of the Subject Property.

A change in land use where the change in use may impact the property's cultural heritage value or interest.

- ▶ No change in land use is being proposed

Land disturbances such as a change in grade that alters soils and drainage patterns that may adversely affect archaeological or cultural heritage resources.

- ▶ Designs will need to account for any alterations in grade to ensure there are no adverse impacts to surrounding structures or city infrastructure.

8. Mitigation Options, Conservation Methods, and Proposed Alternatives

8.1 Mitigation Options and Alternatives

The following mitigation options and alternatives are provided in ranked order based on heritage industry standards:

1. Retain the property in its current configuration and restore it to its 1963 appearance. The interior of the administration building could be retrofit and rented out as office space and the existing warehouse could be updated and partitioned internally for use by multiple clients.
2. Retain the administration building and demolish the warehouse. The interior of the administration building would then be renovated, and the exterior restored to its 1963 configuration. Re-development could then take place surrounding the administration building while retaining the current setback and visibility of the administration building from Steeles Avenue East and Dixie Road.
3. Demolish the extant buildings and facilitate development as proposed. The re-development could conserve the contextual value of the property by establishing a community art installation at the corner of Steeles Avenue East and Dixie Road. This installation would include an interpretive display outlining the history of the development of Bramalea and the role Ford Motor Company of Canada played in the success of Bramalea.

8.1.1 Option 1: Retain and Restore

| Advantages (Pros) | Disadvantages (Cons) |
|--|---|
| <ul style="list-style-type: none"> - No impact to current configuration - Retains existing sightlines - Retention of contextual value | <ul style="list-style-type: none"> - Reduced economic use for the structure - Loss of tax base - Constrains future development opportunities |

8.1.2 Option 2 Retain administration building and demolish warehouse

| Advantages (Pros) | Disadvantages (Cons) |
|--|---|
| <ul style="list-style-type: none"> - Retains the greatest number of character defining heritage attributes while still allowing for some level of re-development - | <ul style="list-style-type: none"> - Reduces developable area - Retains a structure that is not of high cultural heritage value - Limits potential tax base - Minimizes the potential utilization of the property |

8.1.3 Option 3 Undertake re-development as proposed

| Advantages (Pros) | Disadvantages (Cons) |
|--|---|
| <ul style="list-style-type: none">- Creation of useful economic space- Increase tax base- Flexibly of clientele- Opportunity to commemorate and educate the public on the development of Bramalea- Maximizes the utilization of the property | <ul style="list-style-type: none">- Change in existing view scape- Loss of embedded energy- Remediation and retrofit are costly |

8.2 Preferred Mitigation

The preferred mitigation strategy to balance heritage and redevelopment goals is Option 3, the re-development of the property as proposed. This option results in an intensification of use for the property and permits a level of flexibility to accommodate the needs of potential tenants. It also provides an opportunity to educate the public and celebrate the history of the area through the installation of interpretive signage and public display at the corner of Steeles Avenue East and Dixie Road.

8.3 Conservation Methods

The contextual value of the property will be conserved through the installation of a public display at the intersection of Dixie Road and Steeles Avenue East.

Consideration should also be given to utilizing an International Style architectural style on the faces of the structure located closes to the intersection of Dixie Road and Steels Avenue East.

8.4 Current Occupation Status

8000 Dixie Road is currently occupied and utilized as a parts distribution facility by the Ford Motor Company. The second floor of the administration building is currently vacant, but the first floor and lower level continue to be utilized by Ford. Should the property become unoccupied a plan will need to be developed to protect the structures from unauthorized access.

9. Recommendations and Conclusions

Evaluation of 8000 Dixie Avenue against *regulation 9/06* indicates the structure exhibits CHVI. However, the CHVI observed does not warrant heritage designation under Part IV of the OHA. The extant mid-20th century industrial structure is reflective of the International Style of architecture a style which is well represented in the surrounding area. 8000 Dixie was a purpose-built structure commissioned by the Ford Motor Company of Canada to serve as a regional parts distribution centre. The development of the facility was key to the success of Bramalea, now part of the City of Brampton. The structure remains largely as built, with the only notable alteration being the rear addition to the warehouse. The structure stands as an example of the industrial development of the planned community of Bramalea.

While 8000 Dixie is not a candidate for municipal designation, the retention of existing structures is always preferable to demolition as the retention of existing infrastructure helps to retain embodied energy and reduces environmental impacts by diverting waste from landfills.

Given the contextual history of 8000 Dixie Road consideration should be given to the retention and rehabilitation of the existing structure.

The ideal mitigation option would be to retain the extant structure and to incorporate it into the proposed redevelopment. Given that the proposed redevelopment is seeking to maximize the utilization of the Subject Property the retention and integration of the extant structure is impractical and would impose undue hardship on the proposed re-development.

It is recommended that:

1. The feasibility of integration of the extant structure into the redevelopment be assessed.
2. Should integration not be feasible it is recommended that 8000 Dixie Road be subject to salvage during the demolition process. To facilitate this a Salvage and Documentation Plan should be created as the current structure contains many components that could be diverted from landfill and recycled. The Salvage and Documentation Plan can be scoped as additional documentation is not required as the property and existing structures are fully documented as part of this report. Salvageable materials including but not limited to:
 - ▶ Steel framing components
 - ▶ Exterior metal sheeting
 - ▶ Electrical and plumbing components
 - ▶ HVAC system
 - ▶ Industrial engines and generators

If possible salvaged materials should be incorporated into the public display commemorating the contextual value of the property to the development of Bramalea.

3. A demolition permit be issued for 8000 Dixie Road, with a condition of material salvage.

4. A commemoration plan be undertaken, and a commemoration be erected as part of the redevelopment. Commemoration needs to address the origins of any salvaged materials incorporated into the installation, recognition of the importance of Ford Motor Company Canada to the local community and provide a history of the development of Bramalea, Canada's first planned satellite community. It is recommended that commemoration of 8000 Dixie Road be located within sight of the intersection of Dixie Road and Steeles Avenue East. Potential commemoration options include but are not limited to:
 - i. A landscape feature with interpretive signage that is accessible to the public.
 - ii. A sculpture or art installation with descriptive plaque
5. The City of Brampton's municipal register of Cultural Heritage Resources: 'Listed' Heritage Properties be updated, and 8000 Dixie Road be removed.
6. As 8000 Dixie Road is currently occupied the production of a Conservation Plan is not recommended.

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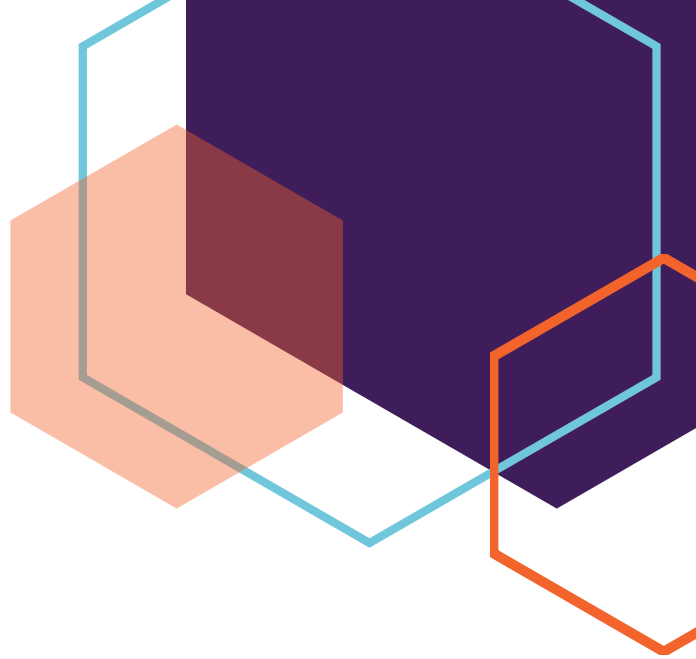
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Appendix A



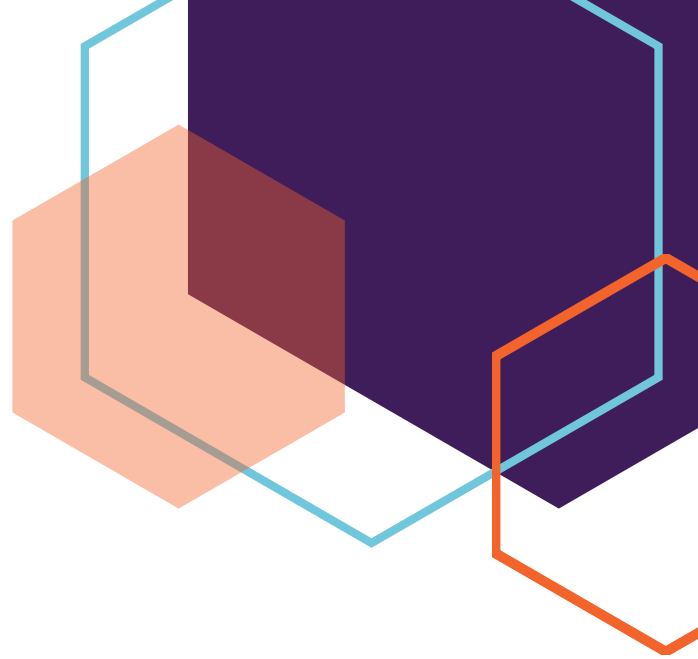
Senior Heritage Specialist – Carla Parslow, PhD, CAHP Member in Good Standing: Dr. Carla Parslow has over 20 years of experience in the cultural heritage resource management (CHRM) industry in Canada. As the President of PHC Inc., Dr. Parslow is responsible for the management of CHRM projects, as well as the technical review and quality assurance of all archaeological and cultural heritage projects completed by PHC. Throughout her career, Carla has managed both large and small offices of CHRM professionals and has mobilized both large (50+) and small (4+) teams of CHRM and Environmental projects offices throughout the province of Ontario. Dr. Parslow has served as either Project Manager or Project Director on hundreds of Archaeological and Cultural Heritage Assessments. Dr. Parslow is a professional member of the Canadian Association of Heritage Professionals (CAHP).

Dr. Parslow is also responsible for the overall quality assurance.

Heritage Specialist – Chris Lemon, B.Sc., Dip. CAHP Member in Good Standing: Chris Lemon is a Cultural Heritage Specialist and Licensed Archaeologist (R289) with 15 years' experience. He received an Honours B.Sc. in Anthropology from the University of Toronto and has completed course work towards an M.A. from the University of Western Ontario. Mr. Lemon has a Diploma in Heritage Carpentry and Joinery and a Certificate in Heritage Planning from Algonquin College. During his career Mr. Lemon has participated in cultural heritage assessments across Ontario as both a Senior Field Director in archaeology and as a Built Heritage Practitioner. Chris's previous experience includes representation on Joint Health and Safety Committees; he is dedicated to maintaining a safety-first focus on all job sites. Chris is a professional member of the Canadian Association of Heritage Professionals (CAHP).

Mr. Lemon is responsible for research, reporting and analysis.

Appendix B



LOT INFORMATION

8000 DIXIE ROAD, BRAMPTON ON L6T 2J7

| COVERAGE % | ACRES | HA | SQ FT | SQ M |
|---|---------|--------|--------------|------------|
| 100.00% | 58.61 | 237.21 | 2,553,262.09 | 237,206.04 |
| METROLINK AREA | 1.23 | 4.97 | 53,528.87 | 4,973.00 |
| TOTAL LOT AREA (EXCLUDING METROLINK ALLOCATED AREA) | 100.00% | 57.25 | 2,519,733.22 | 232,233.04 |
| BUILDING 1 | 11.60% | 6.64 | 26,880.00 | 2,688.00 |
| BUILDING 2 | 22.03% | 13.09 | 52,956.00 | 5,295.60 |
| BUILDING 3 | 5.11% | 2.93 | 11,840.00 | 1,184.00 |
| BUILDING 4 (EXPANSION) | 11.78% | 6.75 | 27,297.00 | 2,729.70 |
| TOTAL BUILDING GFA (EXCLUDING EXPANSION) | 51.60% | 29.54 | 1,286,759.00 | 119,544.00 |
| LANDSCAPING (HARD + SOFT) PAVING | 13.67% | 7.83 | 31,673.40 | 3,167.34 |
| | 33.92% | 19.88 | 865,977.28 | 80,452.00 |

SITE STATISTICS

8000 DIXIE ROAD, BRAMPTON ON L6T 2J7
BUILDING 1

| ZONING | M2-168 | SQ FT | SQ M |
|--|------------------------|----------------------------|----------|
| BUILDING GFA | | 269,333.9 | 26,880.0 |
| OFFICE (INCLUDED IN BUILDING GFA) | | 14,466.7 | 1,344.0 |
| PAVED AREA | | 50,439.7 | 4,686.0 |
| PARKING | REQUIRED | PROVIDED | |
| OFFICE & WAREHOUSE (INC. BARRIER FREE) = 1/170 sqm | 158 | 188 | |
| BARRIER FREE (171-200) | 3-TYPE 'A', 4-TYPE 'B' | 10, 5 TYPE 'A', 5 TYPE 'B' | |
| BIKE PARKING | | 10 | |
| LONG TERM BIKE PARKING (5%) | | 10 | |
| LOADING SPACES | PROVIDED | | |
| TRUCK LEVEL DOOR | | 77 | |
| TRAILER PARK | | 0 | |
| DRIVE-IN | | 2 | |
| LOADING SPACE (3.70Mx18.30M) | | 2 | |
| FIRE TRUCK ROUTE | MIN (m) | PROPOSED (m) | |
| | 6.6 | 6.6 - 21.7 | |
| PARKING STALLS | MIN (m) | PROPOSED (m) | |
| | 2.7 X 5.4 | 2.7 X 5.4 | |
| BUILDING HEIGHT ABOVE GRADE | | 14.31 m | |
| PROPOSED YARD SETBACKS | MIN (m) | PROPOSED (m) | |
| NORTH | 6.0 | N/A | |
| EAST | 6.0 | N/A | |
| SOUTH | 6.0 | N/A | |
| WEST | 6.0 | N/A | |

SITE STATISTICS

8000 DIXIE ROAD, BRAMPTON ON L6T 2J7
BUILDING 2

| ZONING | M2-168 | SQ FT | SQ M |
|--|------------------------|---------------------------|----------|
| BUILDING GFA | | 570,013.6 | 52,956.0 |
| OFFICE (INCLUDED IN BUILDING GFA) | | 28,500.7 | 2,647.8 |
| PAVED AREA | | 50,439.7 | 4,686.0 |
| PARKING | REQUIRED | PROVIDED | |
| OFFICE & WAREHOUSE (INC. BARRIER FREE) = 1/170 sqm | 312 | 317 | |
| BARRIER FREE (301-350) | 4-TYPE 'A', 5-TYPE 'B' | 9, 5 TYPE 'A', 4 TYPE 'B' | |
| BIKE PARKING | | 10 | |
| LONG TERM BIKE PARKING (5%) | | 10 | |
| LOADING SPACES | PROVIDED | | |
| TRUCK LEVEL DOOR | | 77 | |
| TRAILER PARK | | 0 | |
| DRIVE-IN | | 2 | |
| LOADING SPACE (3.70Mx18.30M) | | 2 | |
| FIRE TRUCK ROUTE | MIN (m) | PROPOSED (m) | |
| | 6.6 | 6.6 - 21.7 | |
| PARKING STALLS | MIN (m) | PROPOSED (m) | |
| | 2.7 X 5.4 | 2.7 X 5.4 | |
| BUILDING HEIGHT ABOVE GRADE | | 14.31 m | |
| PROPOSED YARD SETBACKS | MIN (m) | PROPOSED (m) | |
| NORTH | 6.0 | N/A | |
| EAST | 6.0 | N/A | |
| SOUTH | 6.0 | N/A | |
| WEST | 6.0 | N/A | |

SITE STATISTICS

8000 DIXIE ROAD, BRAMPTON ON L6T 2J7
BUILDING 3

| ZONING | M2-168 | SQ FT | SQ M |
|--|------------------------|---------------------------|----------|
| BUILDING GFA | | 299,968.7 | 27,868.0 |
| OFFICE (INCLUDED IN BUILDING GFA) | | 14,998.4 | 1,393.4 |
| PAVED AREA | | 50,439.7 | 4,686.0 |
| PARKING | REQUIRED | PROVIDED | |
| OFFICE & WAREHOUSE (INC. BARRIER FREE) = 1/170 sqm | 164 | 185 | |
| BARRIER FREE (171-200) | 3-TYPE 'A', 4-TYPE 'B' | 8, 4 TYPE 'A', 4 TYPE 'B' | |
| BIKE PARKING | | 10 | |
| LONG TERM BIKE PARKING (5%) | | 10 | |
| LOADING SPACES | PROVIDED | | |
| TRUCK LEVEL DOOR | | 47 | |
| TRAILER PARK | | 0 | |
| DRIVE-IN | | 2 | |
| LOADING SPACE (3.70Mx18.30M) | | 2 | |
| FIRE TRUCK ROUTE | MIN (m) | PROPOSED (m) | |
| | 6.6 | 6.6 - 21.7 | |
| PARKING STALLS | MIN (m) | PROPOSED (m) | |
| | 2.7 X 5.4 | 2.7 X 5.4 | |
| BUILDING HEIGHT ABOVE GRADE | | 14.31 m | |
| PROPOSED YARD SETBACKS | MIN (m) | PROPOSED (m) | |
| NORTH | 6.0 | N/A | |
| EAST | 6.0 | N/A | |
| SOUTH | 6.0 | N/A | |
| WEST | 6.0 | N/A | |

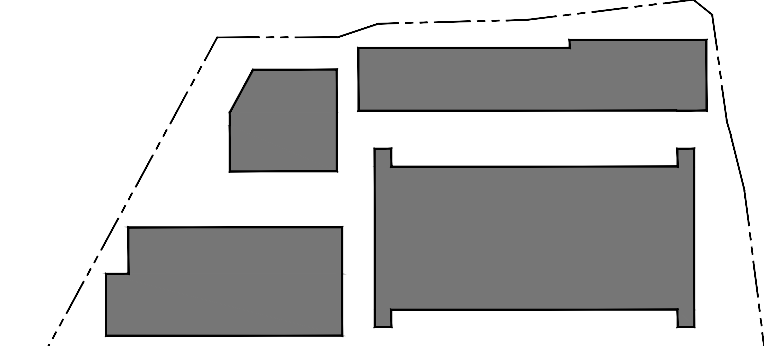
SITE STATISTICS

8000 DIXIE ROAD, BRAMPTON ON L6T 2J7
BUILDING 4

| ZONING | M2-168 | SQ FT | SQ M |
|--|------------------------|--------------|----------|
| BUILDING GFA | | 127,444.7 | 11,840.0 |
| OFFICE (INCLUDED IN BUILDING GFA) | | 6,372.2 | 592.0 |
| PAVED AREA | | 50,439.7 | 4,686.0 |
| PARKING | REQUIRED | PROVIDED | |
| OFFICE & WAREHOUSE (INC. BARRIER FREE) = 1/170 sqm | 70 | 54 | |
| BARRIER FREE (171-200) | 1-TYPE 'A', 2-TYPE 'B' | 2 | 3 |
| BIKE PARKING | | 10 | |
| LONG TERM BIKE PARKING (5%) | | 10 | |
| LOADING SPACES | PROVIDED | | |
| TRUCK LEVEL DOOR | | 23 | |
| TRAILER PARK | | 14 | |
| DRIVE-IN | | 2 | |
| LOADING SPACE (3.70Mx18.30M) | | 2 | |
| FIRE TRUCK ROUTE | MIN (m) | PROPOSED (m) | |
| | 6.6 | 6.6 - 21.7 | |
| PARKING STALLS | MIN (m) | PROPOSED (m) | |
| | 2.7 X 5.4 | 2.7 X 5.4 | |
| BUILDING HEIGHT ABOVE GRADE | | 14.31 m | |
| PROPOSED YARD SETBACKS | MIN (m) | PROPOSED (m) | |
| NORTH | 6.0 | N/A | |
| EAST | 6.0 | N/A | |
| SOUTH | 6.0 | N/A | |
| WEST | 6.0 | N/A | |

1. VERIFY ALL DIMENSIONS PRIOR TO CONSTRUCTION.
2. DO NOT SCALE DRAWINGS.
3. REPORT ALL DISCOVERIES OF ERRORS, OMISSIONS OR DISCREPANCIES TO THE ARCHITECT OR DESIGN ENGINEER AS APPLICABLE.
4. USE ONLY LATEST REVISED DRAWINGS OR THOSE THAT ARE MARKED "ISSUED FOR CONSTRUCTION".
5. THE DRAWINGS ARE THE PROPERTY OF JRI ARCHITECTS INC. AND MUST BE RETURNED ON COMPLETION OF THE PROJECT. ANY UNAUTHORIZED USE IS PROHIBITED.

NOTE: SURVEY INFORMATION ILLUSTRATED ON THIS DRAWING WAS TAKEN FROM CITY FILE: XXX. PREPARED BY XXX

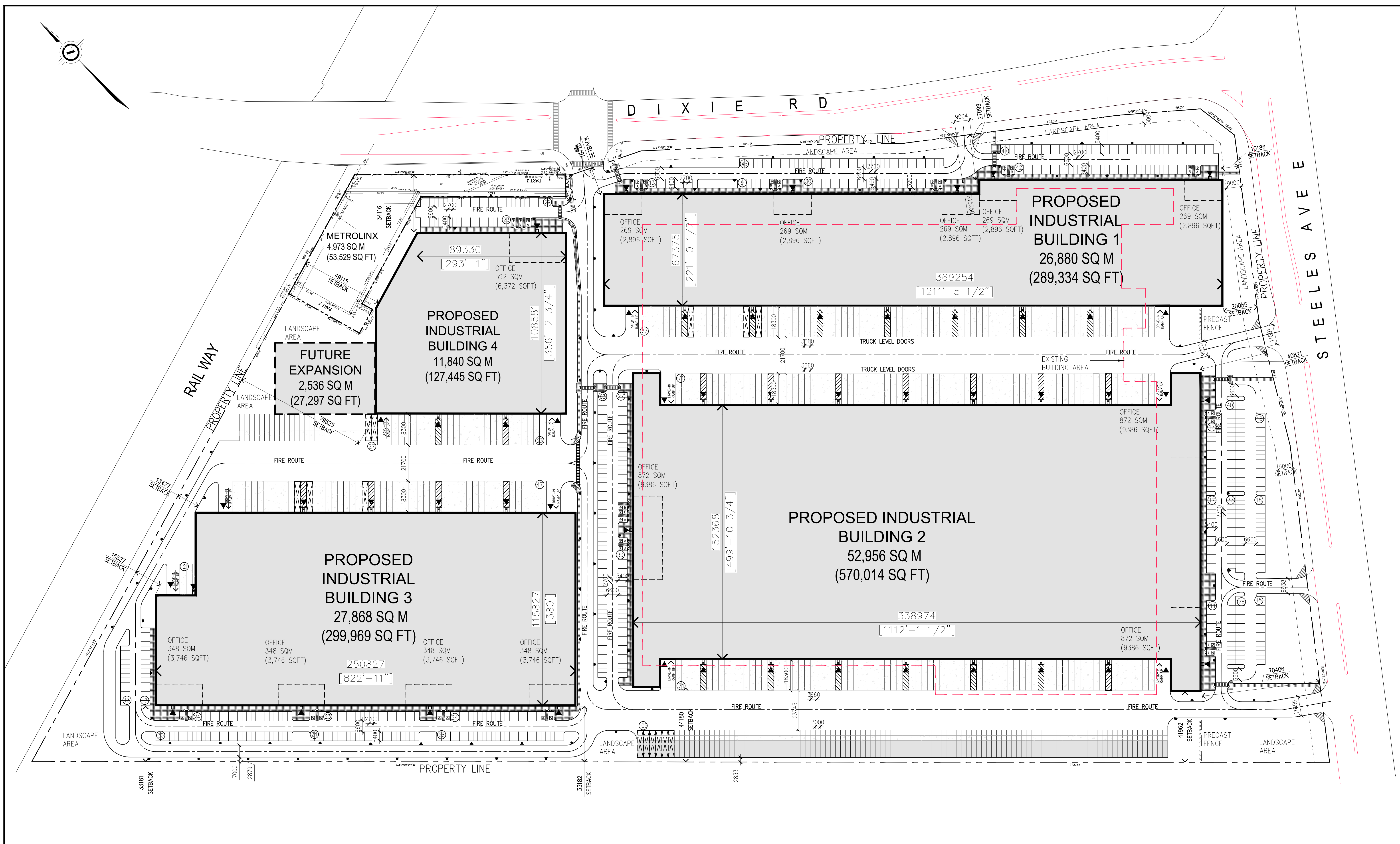


KEY PLAN

LOT INFORMATION AND MEASUREMENTS ARE BASED ON DERIVED FROM VARIOUS ONLINE SOURCES. ACTUAL/OFFICIAL SURVEY RESULTS MAY VARY.

LEGEND

EXISTING BUILDING AREA



| | | | |
|-----|-------------|-----------------------------|-------------|
| 8. | | | |
| 7. | | | |
| 6. | | | |
| 5. | | | |
| 4. | | | |
| 3. | | | |
| 2. | AUG 13/2021 | ISSUED TO CLIENT/CONSULTANT | |
| 1. | AUG 09/2021 | ISSUED TO CONSULTANT | |
| NO. | DATE | REVISIONS | APPROVED BY |



JRI
ARCHITECTS

4 PRINCE GEORGE DRIVE, ETOBICOKE, ONTARIO M9A 1X8



PANATTONI
185 The West Mall, Suite 860
Toronto, ON M5C 5L

INDUSTRIAL DEVELOPMENT

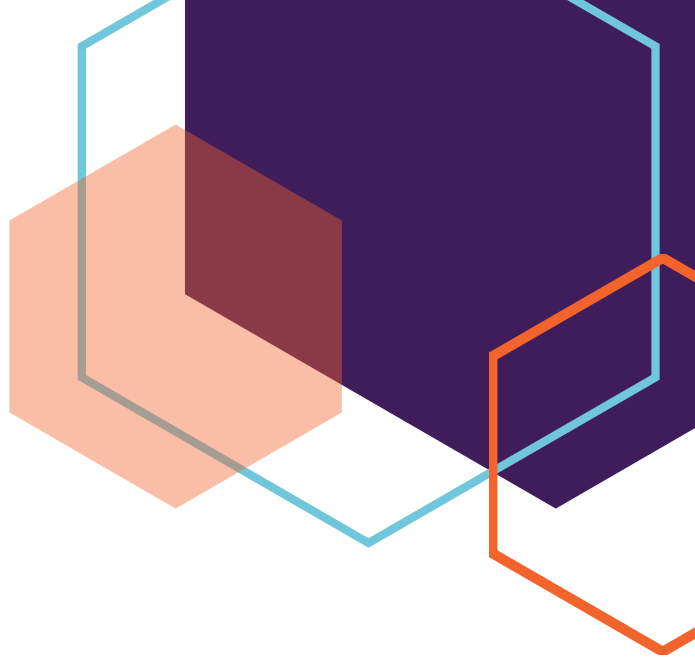
8000 DIXIE ROAD, BRAMPTON ON L6T 2J7

OVERALL SITE PLAN

| | |
|---------|----------|
| DRAWN | RA |
| CHECKED | JR |
| SCALE | AS SHOWN |
| DATE | MAY 2021 |

| | |
|----------------|----------------|
| PROJECT NUMBER | DRAWING NUMBER |
| 2021-01 | A3.0 |

Appendix C



BRAMPTON HERITAGE INVENTORY

LISTING CANDIDATE SUMMARY



Ford Motor Company, Brampton Sales and Parts Distribution Centre

8000 Dixie Road
(West side of Dixie Road at Steeles)

February 2005

| | |
|---|--|
| PROPERTY NAME | Ford Motor Company, Brampton Sales and Parts Distribution Centre |
| MUNICIPAL ADDRESS | 8000 Dixie Road (West side of Dixie Road at Steeles) |
| LEGAL DESCRIPTION | PLAN P676 LOT BLK A , C3E1 WARD 8 |
| WARD | |
| ROLL NUMBER | 2110090023060000000 |
| PIN | 141620061 |
| STATEMENT OF SIGNIFICANCE | <p>This industrial building dates to the 1960s and is a good local example of the International Style, a modernist architectural style. The building is essentially unchanged and is highly representative of the emerging heritage significance found in Bramalea - Canada's first planned suburban community. Bramalea was planned with a mix of residential neighbourhoods, parks and recreational facilities, church and a large industrial base, such as this Ford Motor facility. Ford has its national parts distribution centre in Brampton, Ontario, with five other distribution centres and six regional sales offices across the country.</p> <p>Architecturally the low rise building clad in white brick is distinguished by exposed structural columns that stand out from the curtain wall and mullions which create a grid effect as they cross the horizontal lines of the windows.</p> <p>The building is a architectural similar to the J.A. Russell Building at the University of Manitoba (1959), the Ortho Plant in Don Mills and the Winnipeg Civic Centre.</p> <p>The intersection of Dixie and Steeles was once known as "Fraser's Corners".</p> |
| CRITERIA GRADE | B |
| CONSTRUCTION OR CREATION DATE | 1962 circa |
| TYPE OF HERITAGE RESOURCE(S) archaeological site district-area-complex building cemetery-burial site structure-object historic site cultural heritage landscape | Building |
| CURRENT USES AND FUNCTIONS | Industrial |
| SUBMISSION SOURCE | Heritage Resources Sub-Committee |
| EVALUATION DATE | February 1, 2005 |
| EVALUATION BY | Jim Leonard |
| SUBCOMMITTEE DATE | |

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| BHB DATE | |
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PHOTOGRAPHS:



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883 St. Clair Avenue West, Rear, Toronto, ON, M6C 1C4

Telephone: [647-348-4887](tel:647-348-4887)

Email: admin@phcgroup.ca

Website: www.phcgroup.ca